

Ordinance No. 18-2009

**An ordinance of the Mayor and Board of Alderman of the Town of Dover
County of Morris and the State of New Jersey
Amending Chapter 236, LAND USE AND DEVELOPMENT, ARTICLE IV, Zoning,
of the Code of the Town of Dover**

BE IT ORDAINED by the Mayor and Board of Alderman of the Town of Dover, County of Morris and state of New Jersey as follows:

Section 1

Chapter 236, LAND USE AND DEVELOPMENT, ARTICLE IV, Zoning, 236-11. Zoning Districts and Zoning Map, Paragraphs A. and B. is replaced with the following:

§ 236-11. Zoning Districts and Zoning Map.

- A. Zoning districts. For the purpose of this chapter, the Town of Dover is hereby divided into zoning districts as follows:
- R-1..... Single-Family
 - R-1S..... Steep Slope Single-Family
 - R-2..... Single-Family
 - R-3..... Double-Family
 - R-3A Double-Family/Rooming House
 - R-4..... Multifamily
 - C-1..... Retail Commercial
 - C-2..... General Commercial
 - C-3..... Light Industrial-Commercial
 - D1..... Station Area
 - D2..... Blackwell Street Historic
 - D3..... East Blackwell Business
 - D4..... South Downtown
 - IND Industrial
 - IND/OP Industrial-Office Park
 - RAD Redevelopment Area District
 - BHRPA..... Bassett Highway Redevelopment Plan Area
- B. Zoning Map. The boundaries of these districts are hereby established as shown on the map entitled "Zoning Map, Town of Dover, Morris County, New Jersey," prepared by Michael A. Hantson, PE, PP, CME – Town Engineer & Planner, dated November 2009, which accompanies and is hereby made part of this chapter.

Section 2

Chapter 236, LAND USE AND DEVELOPMENT, ARTICLE IV, Zoning, is supplemented with the following:

236-17.1 Downtown Districts.

This section of the Land Development Ordinance establishes the 'Downtown District' and provides a Form-Based Code (Code) for Town of Dover's downtown area (Downtown). Where the provisions of the Land Use and Development Code and §236-17.1 are inconsistent, the regulations of §236-17.1 shall apply. Any graphical reference to District boundaries shown in §236-17.1 shall be superceded by the boundaries adopted by the Zoning Map – Town of Dover as they may be lawfully changed from time to time.

A companion document entitled, "Form Based Code – Town of Dover," prepared by Heyer, Gruel & Associates dated November 2009, contains graphical and pictorial examples of the application of this code and is encouraged to be used for reference not regulatory purposes.

A. Intent

The primary intent of this section of the Land Development Ordinance is to establish the 'Downtown District' and create a regulatory framework to implement the 2006 Transit Oriented Development Plan (TOD Plan) for the Downtown Area.

The Downtown Districts comprise Subareas 1, 3 and 5 of the TOD Plan, and include the train station and its surrounding properties, retail uses on Blackwell Street, and the surface parking lots owned by the Town and NJ Transit. The zone boundaries of the Downtown Districts generally follow the Subarea boundaries established in the TOD Plan (See Zoning Map); and the Dover Historic District boundary. The key principles of each Subarea, and the character defining elements of the Historic District are translated into district goals of the respective Downtown District.

The Downtown District is composed of D1 STATION AREA DISTRICT, D2 BLACKWELL STREET HISTORIC DISTRICT, D3 EAST BLACKWELL BUSINESS DISTRICT, and D4 SOUTH DOWNTOWN DISTRICT, which form the basis of a comprehensive change to the zoning within the Downtown. The land development regulations for the Downtown Districts are "form-based," and place greater emphasis on the form of a building, rather than the use of a development site (or sites) and are meant to achieve predictable physical outcomes. They regulate the form, placement and design of private buildings in order to shape the public realm – the space between buildings. These form-based regulations will facilitate new development in Dover while remaining respectful of the historic integrity of the Town.

Specific objectives of the Downtown Districts are:

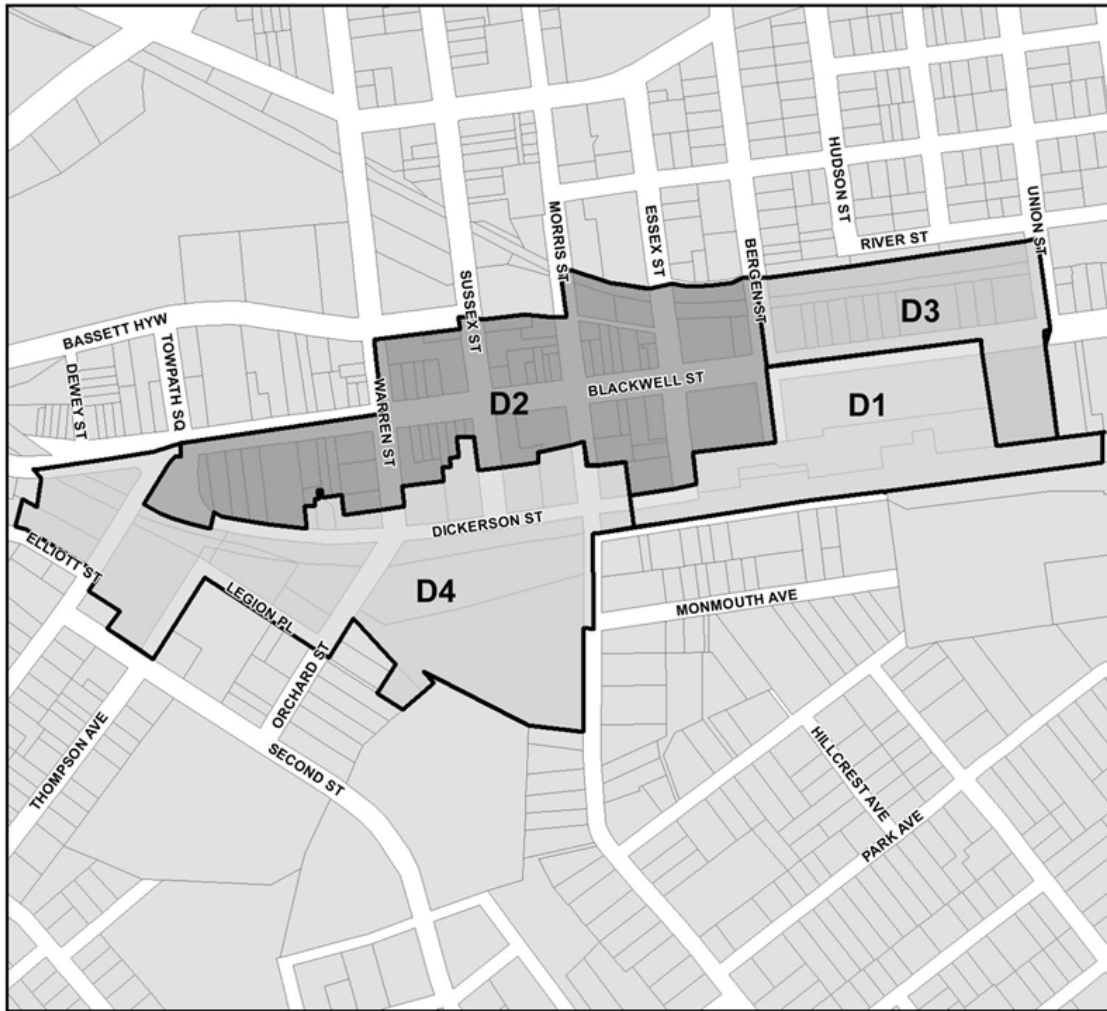
- (1) To create a framework for the construction of new buildings and renovation of existing buildings with a Form-Based approach and to reinforce the positive elements of the Downtown area.
- (2) To define distinctive street profile specifications considering the needs of various groups of users - businesses, pedestrians, motorists, bicyclists, and transit riders - while creating a public realm that is active, comfortable and attractive.
- (3) To provide design standards for enhancing the Downtown and guide the form and architecture of new development to be complementary to its historic character and context.

B. Organization

This section of the Land Development Ordinance regulates development by the provision of: District Regulating Map - Designated Districts (Regulating Map 1), District Regulating Map - Designated Overlays (Regulating Map 2), Height Regulating Map, Downtown Districts, Building Types, Frontage Types, Street Profile Specifications, Architectural Standards, and Definitions.


- (1) District Regulating Map 1 – Downtown Districts (Regulating Map 1)



The Downtown District boundaries are established by Zoning Map but are indicated herein for reference by District Regulating Map 1 - Downtown Districts (Regulating Map 1), Figure 236-17.1- A. Future boundaries may change therefore the Zoning Map should be referred to for regulatory purposes.



DISTRICT REGULATING MAP 1 - DOWNTOWN DISTRICTS

Downtown Districts

-  D1 - Station Area District
-  D2 - Blackwell Street Historic District
-  D3 - East Blackwell Business District
-  D4 - South Downtown District

 Data Sources: NJDEP and the Town of Dover

 October 2009

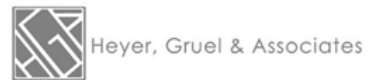


Figure 236-17.1- B

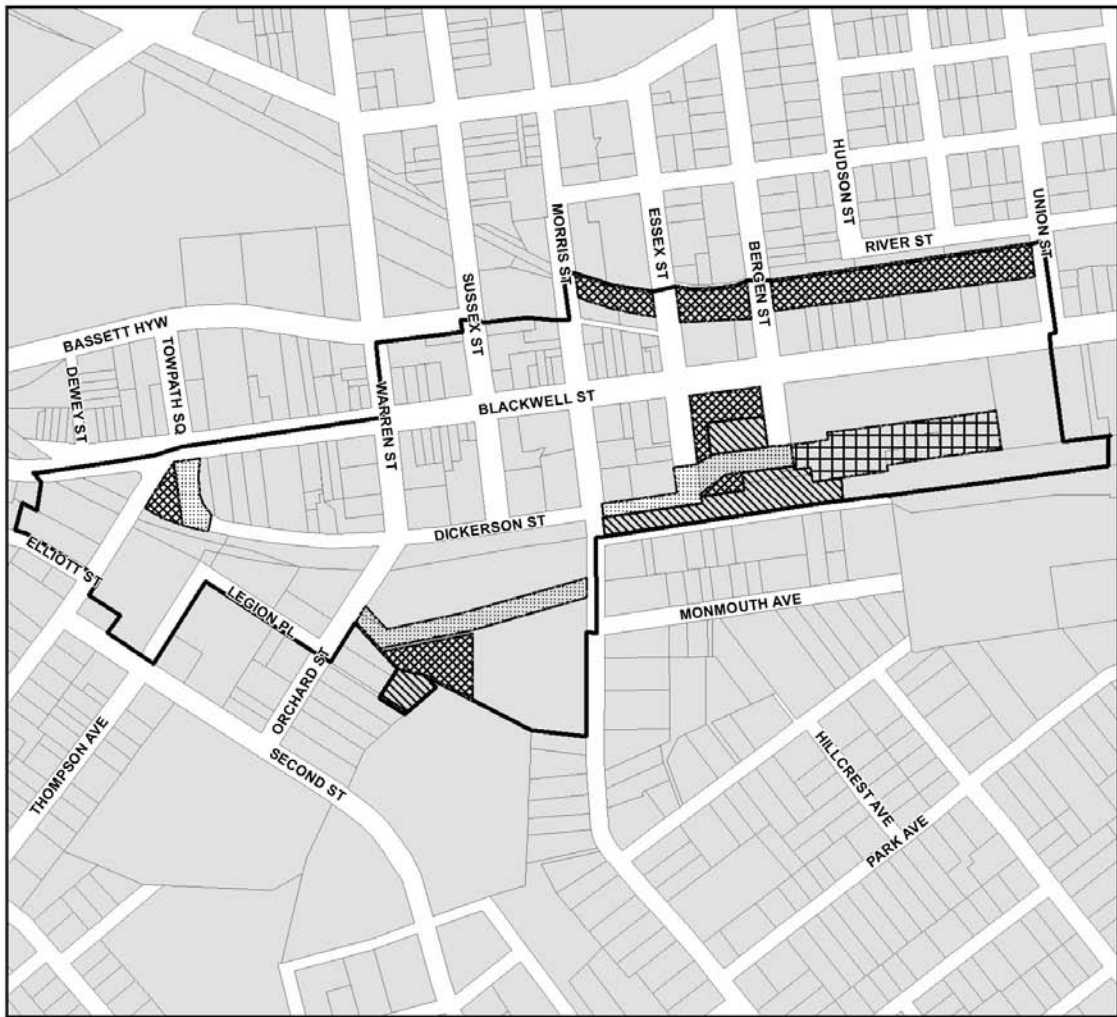
The following Downtown Districts are established:

- (a) D1 Station Area District
- (b) D2 Blackwell Street Historic District
- (c) D3 East Blackwell Business District
- (d) D4 South Downtown District

(2) District Regulating Map 2 - Civic Spaces and Public/Quasi-Public Overlays (Regulating Map 2)




The Designated Overlays are established by the District Regulating Map 2 – Civic Spaces and Public/Quasi-Public Overlays (Regulating Map 2), Figure 236-17.1- C. Regulating Map 2 shows the location of designated civic buildings, civic spaces, new rights-of-way, and public parking. The purpose of this map is to reserve specific areas within the downtown for the creation of public improvements including civic spaces, civic buildings, new rights-of-way and public parking. Development within these overlays shall be limited to the creation of civic spaces and public/quasi-public buildings, irrespective of the provisions of Downtown Districts. Development in the Designated Overlays is regulated as follows:

- (a) Designated Civic Space: The primary use of land within this overlay shall be park, plaza, greenway or other types of public space. No building or parking may be constructed within the overlay.
- (b) Designated Civic Building: Provides for the conservation and construction of civic buildings. The primary use of land within this overlay shall be a civic building. The only Building Type permitted within the overlay shall be the “Civic Building”.
- (c) Designated New Right-Of-Way: The primary use of land within this overlay shall be a public right-of-way, designed in accordance with the Street Type Specifications of this Code. The “New Right-Of-Way Overlay” along the Dover Train Station frontage shall be designed in accordance with the recommendations of the TOD Plan, and shall incorporate enhanced pedestrian improvements and a “kiss-n-ride” amenity.
- (d) Designated Public Parking: The primary use of land within this overlay shall be surface or structured parking facilities for the provision of public parking. Liner Buildings, as defined in the Building Type regulations of this Code, may be provided to shield parking areas from view.



**DISTRICT REGULATING MAP 2 -
CIVIC SPACES AND PUBLIC/QUASI-PUBLIC OVERLAYS**

Designated Overlays

-  Civic Building
-  Civic Space
-  New & Existing Right-Of-Way
-  Public Parking
-  Downtown District Zone Boundary



0 400 800 Feet

Data Sources: NJDEP and the Town of Dover

October 2009



Heyer, Gruel & Associates

Figure 236-17.1- D

(3) District Regulating Map 3 – Building Heights

The District Regulating Map3 – Building Heights, Figure 236-17.1- E, illustrates the maximum building height in stories and feet permitted in each Downtown District. The height regulations shall apply to new construction as well as the future demolition and replacement. The height regulations shall not apply to any existing buildings within the Historic District. Any deviation from the Height Regulating Map requires a variance either from 40:55D-70.c or 40:55D-70.d in accordance with the Municipal Land Use Law.

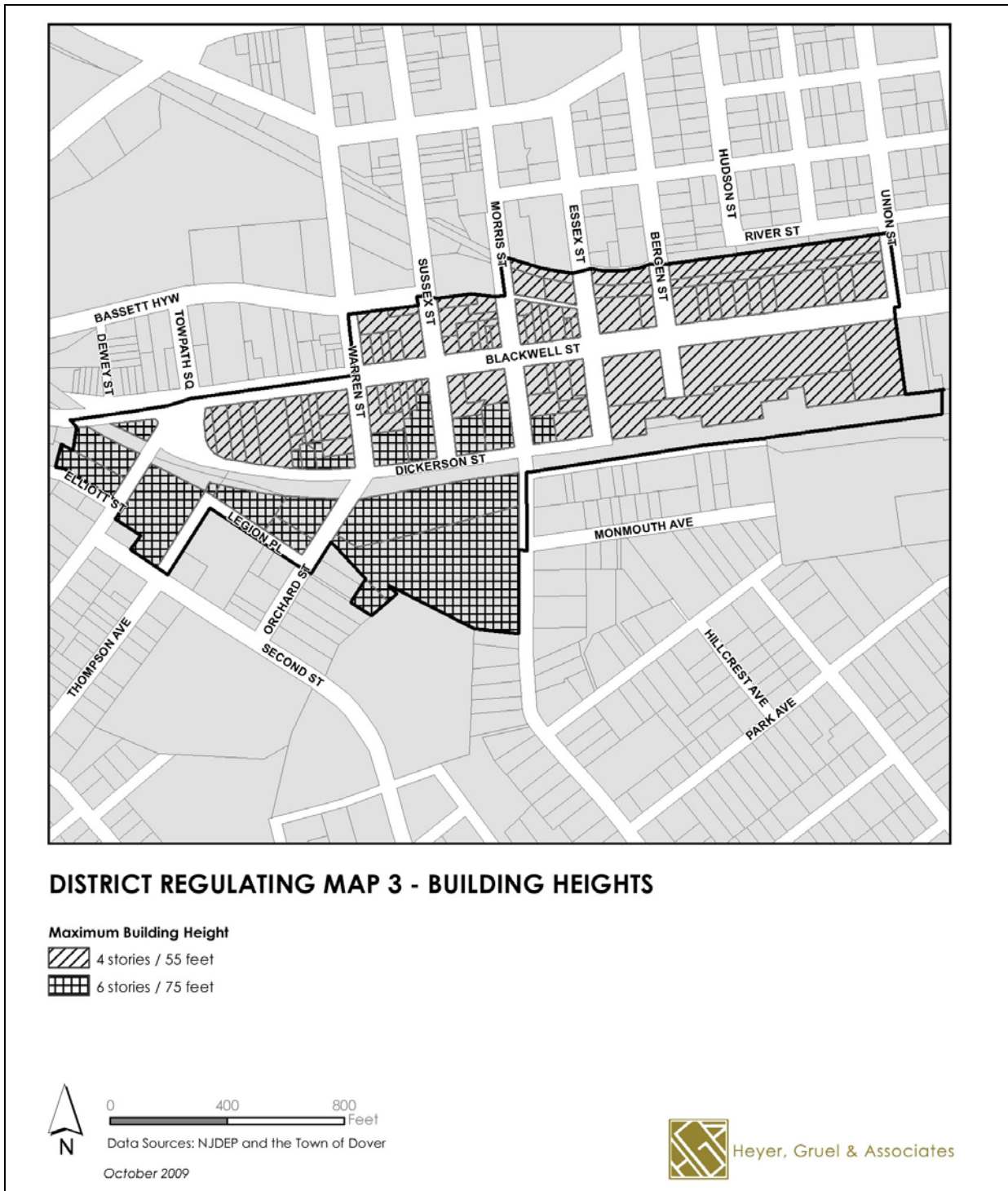


Figure 236-17.1- F

(4) Downtown Districts

This subsection establishes the primary goals and the general development regulations including permitted uses, lot sizes, setbacks, building heights, and the location of parking in each Downtown District. Permitted uses are provided by Downtown District and Building Type, in the Permitted Uses by Building Type Summary Table. Any deviation from the principal permitted uses requires a “d” variance.

(5) Building Types

Based on the predominant character and anticipated development patterns, as detailed in the TOD Plan, Building Types are specified for each of the Downtown Districts. Building Types are either allowed or prohibited based on the intended character of a District – they are the character defining elements of a District. Hence, any deviation from the permitted Building Types requires a “d” variance.

Building Type regulations, which focus on achieving the desired and essential characteristics, are specified in this subsection. This layer of regulation is designed to maintain the rich variety of buildings and streetscapes in the Downtown. The permitted uses, lot sizes, setbacks, building heights, and the location of parking are specified in the Downtown District regulations. The building type regulations do not apply to any existing buildings within the Historic District. A “Zone Application key Map” is provided as a convenience for each Building Type. This key map shows the Downtown Districts or portions of Downtown Districts where the Building Type is permitted.

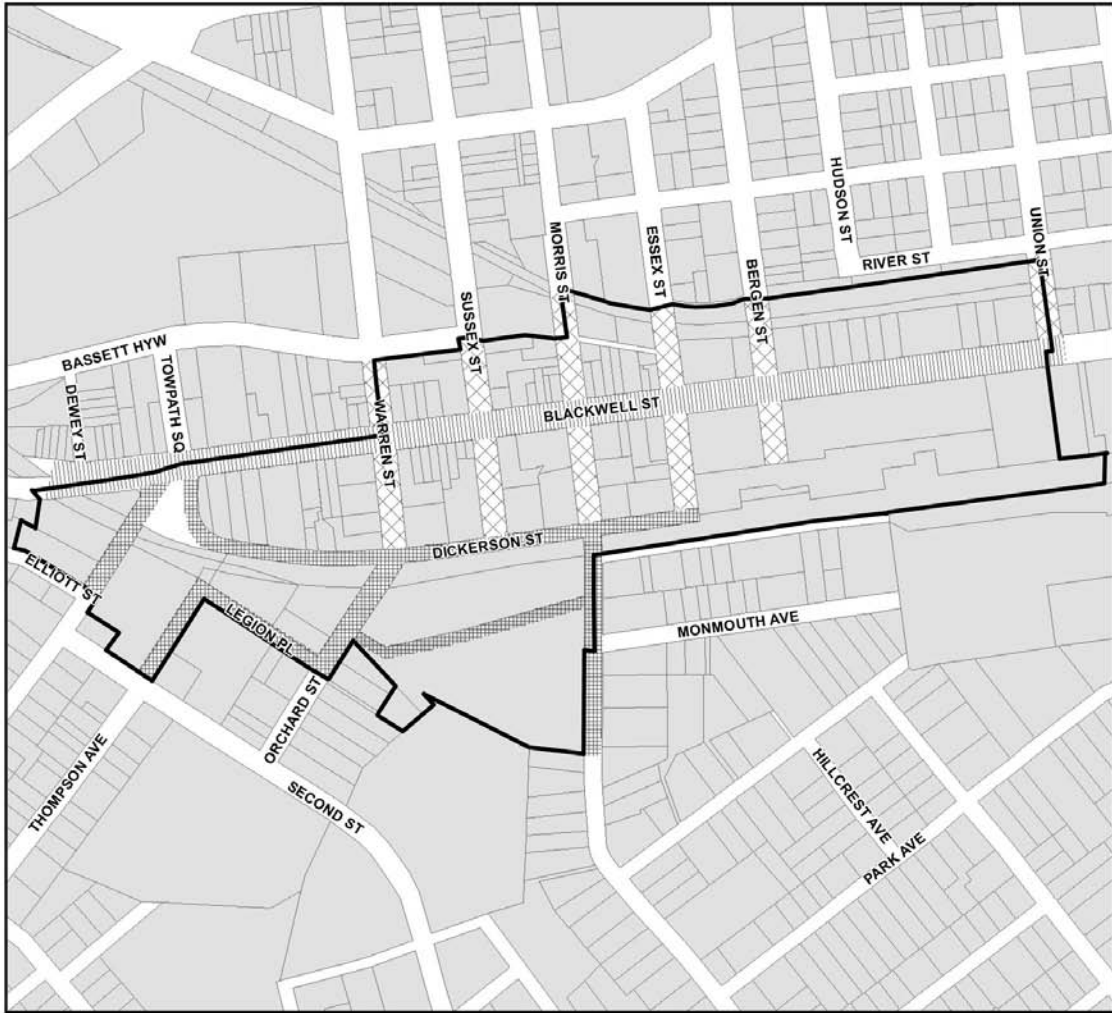
In case of a conflict between regulations for Downtown Districts and the specific requirements of a Building Type, the Building Type requirements shall take precedence.

(6) Frontage Types

The permitted Frontage Types include: shopfront & awning; stoop; terrace; and arcade. Building Frontage essentially defines the way a building relates to the public realm. Frontage Types are intended to regulate the ground-level frontage of a building, along a public street or a public space and to ensure that its interface with the public realm and the transition between the two are detailed properly. In this Code, Frontage Types are permitted by Downtown District, Building Type as well as the building use provided at the street level. Any deviation from the design requirements of a permitted frontage type shall constitute a ‘c’ variance.




(7) Street Profile Specifications and District Regulating Map 4 - Street Types

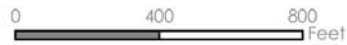
Streets within the Downtown are classified as Blackwell Street, Avenue ‘A’, Avenue ‘B’, based on the function and width of right-of-way. The District Regulating Map 4 - Street Types, Figure 236-17.1- G, illustrates the primary Street Types within the Downtown. The Street Profile Specifications illustrate the typical configuration of streets and street segments within the Downtown. These specifications address vehicular lane widths, sidewalks, on street parking, tree planting areas, bike lanes, and placement of street furniture and lighting. The respective street profile diagrams guide the street types.



DISTRICT REGULATING MAP 4 - STREET TYPES

Street Type

-  Blackwell Street
-  Avenue A
-  Avenue B



Data Sources: NJDEP and the Town of Dover
October 2009



Heyer, Gruel & Associates

Figure 236-17.1- H

(8) Architectural Standards

The Architectural Standards regulate the architectural elements of a building and set the parameters for configurations, styles, construction techniques, and desired materials. The standards also regulate the design of various elements of the buildings within the Districts including facades, roofs, fences, walls, and awnings. From a regulatory perspective, they shall be considered design standards and deviations shall be considered design standard waivers.

(9) Sustainability/"Green Design" Regulations for Downtown Districts

This section of the Code provides the necessary steps in creating a sustainable, mixed use, pedestrian friendly environment that protects and enhances natural resources while providing individuals and families with safe, healthy and comfortable places to live, work, and recreate.

(10) Streetscape Standards

The Streetscape Standards are meant to guide public investment in streetscape improvements, and to assist property owners and developers in designing their own building's relation to the public realm. These standards specify the allowable planters, trashcans, bike racks, bus stops, lighting, and paving materials.

(11) Definitions

This section contains a glossary of technical terms that are specific to the Downtown Districts. Article II Definitions and Word Usage, shall be the primary source of all other definitions. In case of a conflict in usage of words, the definitions in this Code shall supersede Article II.

C. Code Navigation

The Code for the Downtown has been designed to be user-friendly. The provisions of the Downtown Districts can be verified in seven simple steps:

- (1) Locate your parcel on the District Regulating Map 1 – Designated Districts (Figure 236-17.1- I) and identify the Downtown District in which it is located. Also, determine if your parcel is situated in the Historic District (as defined by Section 236-96.7 in article VIIIA of the Land Development Ordinance).
- (2) Locate your parcel on the District Regulating Map 2 – Civic Spaces and Public/Quasi-Public Overlays (Figure 236-17.1- J) and determine if your parcel is situated in a Designated Overlay.
- (3) Review the Downtown Districts subsection (236-17.1.D), to identify the permitted uses, lot sizes, setbacks, building heights, and the location of parking.
- (4) Use the District Regulating Map 3 – Building Heights (Figure 236-17.1- K), to determine the maximum building height for the parcel.
- (5) Review the Building Types subsection (236-17.1.E) to determine the Building Types that can be built on the parcel. Once a building type is identified, determine the requirements that are specific to the building type.
- (6) Review the Frontage Types subsection (236-17.1.F), to determine the frontage types allowed. Once a frontage type is identified, determine the requirements that are specific to the frontage type.
- (7) Review the General Regulations for Downtown Districts (236-17.1.H), to determine if any additional regulations may be applicable.
- (8) Review the Architectural Standards (236-17.1.I) to determine the applicable architectural regulations.
- (9) If the parcel is located within the Historic District, refer to Historic District requirements specified in Section 236-96.7 in Article VIIIA of the Land Development Ordinance.

Table 236-17.1-I is a list of regulatory maps and summary tables:

Regulatory Maps & Summary Tables			
		Section #	Table / Figure No.
MAP	District Regulating Map 1 – Downtown Districts Note: Refer to Zoning Map for Boundaries	Regulating Maps	Figure 236-17.1- L
	District Regulating Map 2 – Civic Spaces and Public/Quasi-Public Overlays	Regulating Maps	Figure 236-17.1- M
	District Regulating Map 3 – Building Heights	Regulating Maps	Figure 236-17.1- N
	District Regulating Map 4 – Street Types	Regulating Maps	Figure 236-17.1- O
SUMMARY TABLE	Building Types By Downtown District Summary Table	Downtown Districts	Table 236-17.1- 1
	Permitted Uses by Building Type Summary Table	Downtown Districts	Table 236-17.1- 2
	Allowable Frontage Types	Frontage Types	Table 236-17.1- 3

Table 236-17.1- 4

D. Downtown Districts

(1) D1 Station Area District

(a) District Purpose / Goal

- [1] To create a civic core of the Town with the Dover Train Station as its focus.
- [2] To create a public plaza and an outdoor dining space at the train station.
- [3] To continue the presence of multi-family residential housing close to the train station.
- [4] To create an opportunity for public and commuter parking.

(b) Permitted Uses

[1] Principal Uses

In accordance with the Permitted Uses By Building Type Summary Table.

[2] Accessory Uses

Uses accessory and incidental to principal permitted uses such as parking, loading, on-site storage, apartment common areas, and trash recycling areas.

[3] Additional Use Regulations:

[a] Public parking lots (and structures) are permitted as the primary use of property in the “public parking” overlay areas shown on District Regulating Map 2

[b] Multi-family residential housing is permitted as a principal use only in Block 1213, Lot 3

(c) Placement Of Accessory Uses With Respect to Principal Building

Parking, Loading, Trash and On-Site Storage shall be located in the rear or interior only

(d) Building Form Standards

- [1] The maximum wall plane width (Vertical Rhythm) shall be 50 feet
- [2] The maximum single tenant commercial area shall be 10,000 square feet.

[3] The minimum residential unit size shall be 800 square feet.

(e) Setbacks

[1] Setbacks shall be regulated by building type.

(f) Building Height

[1] The minimum building height shall be regulated by building type

[2] The maximum building height shall be as indicated in the District Regulating Map 3 – Building Heights

[3] There is no vertical setback requirements from a public street

(g) Coverage Requirements

[1] Maximum Building Coverage shall not exceed 85 percent

[2] Maximum Impervious Converge shall not exceed 90 percent

(h) Permitted Building Types

Only the following Building Types are permitted

[1] Commercial Block

[2] Liner Building

[3] Townhouse

[4] Civic Building

[5] Multi-Family Building

(2) D2 Blackwell Street Historic District

(a) District Purpose / Goal

[1] To provide for the renovation and conservation of historic buildings within the Dover Historic District.

[2] To create a strong street edge of mixed-use buildings with retail on ground floor with residential and commercial uses permitted by right in the upper floors.

[3] To create opportunities for retail, office, theaters, and eating/dining establishments.

(b) Permitted Uses

[1] Principal Uses

Permitted uses within existing structures shall be in accordance with the permitted uses provided in the Permitted Uses By Building Type Summary Table (All Existing Buildings)

[2] Accessory Uses

Uses accessory and incidental to principal permitted uses such as parking, loading, on-site storage, apartment common areas, and trash recycling areas.

(c) District Regulations

[1] All buildings within the Historic District shall be retained and restored as required by Article VIIA Historic Preservation.

[2] Where existing buildings are restored or re-used in their current state, the principal structure of all existing buildings within the District shall be considered a conforming structure. No bulk requirements are provided within this District.

- [3] In case of new construction or demolition (where existing buildings are replaced by new structures), the district, use, building type, and frontage regulations of D3 East Blackwell Business District shall apply.
- [4] In case of new construction or demolition (where existing buildings are replaced by new structures), the architectural standards of this Code shall apply.
- [5] The provisions of Article VIIA Historic Preservation, shall apply to all lots within D2 Blackwell Street Historic District.

(3) D3 East Blackwell Business District

(a) District Purpose / Goal

- [1] To build upon the characteristic elements of the Town's Historic District, and function as an extension of the D2 Blackwell Street Historic District.
- [2] To create a strong street edge of mixed-use buildings with retail on ground floor with residential and commercial uses permitted by right in the upper floors.
- [3] To create opportunities for retail, office, theaters, and eating/dining establishments

(b) Permitted Uses

[1] Principal Uses

In accordance with the Permitted Uses By Building Type Summary Table.

[2] Accessory Uses

Uses accessory and incidental to principal permitted uses such as parking, loading, on-site storage, apartment common areas, and trash recycling areas.

(c) Placement Of Accessory Uses With Respect to Principal Building

Parking, Loading, Trash and On-Site Storage shall be located in the rear or interior only

(d) Building Form Standards

- [1] The maximum wall plane width (Vertical Rhythm) shall be 50 feet
- [2] The maximum single tenant commercial area shall be 10,000 square feet.
- [3] The minimum residential unit size shall be 800 square feet.

(e) Setbacks

- [1] Setbacks shall be regulated by building type.

(f) Building Height

- [1] The minimum building height shall be regulated by building type
- [2] The maximum building height shall be as indicated in the District Regulating Map 3 – Building Heights
- [3] There is no vertical setback requirements from a public street

(g) Coverage Requirements

- [1] Maximum Building Coverage shall not exceed 70 percent
- [2] Maximum Impervious Converge shall not exceed 70 percent

(h) Permitted Building Types

Only the following Building Types are permitted

- [1] Commercial Block

- [2] Corner Buildings
- [3] Civic Building
- (4) D4 South Downtown District
 - (a) District Purpose / Goal
 - [1] To create mixed-use development, that capitalizes on its proximity to the train station, and is a complementary extension of the existing downtown.
 - [2] To capitalize on the existing park/recreation space (Crescent Field)..
 - [3] To create new multi-family residential housing close to the train station.
 - [4] To preserve and enhance the public and commuter parking facilities.
 - (b) Permitted Uses
 - [1] Principal Uses
 - In accordance with the Permitted Uses By Building Type Summary Table.
 - [2] Accessory Uses
 - Uses accessory and incidental to principal permitted uses such as parking, loading, on-site storage, apartment common areas, and trash recycling areas.
 - [3] Additional Use Regulations:
 - [a] Multi-Family and Townhouse building types are only permitted west of Orchard Street.
 - [b] With respect to the New Jersey Transit maintenance facility in Block 510 Lot 6, should New Jersey Transit choose to change or relocate this facility, it must be relocated to a suitable location outside the Downtown Districts.
 - (c) Placement Of Accessory Uses With Respect to Principal Building
 - Parking, Loading, Trash and On-Site Storage shall be located in the rear or interior only
 - (d) Building Form Standards
 - [1] The maximum wall plane width (Vertical Rhythm) shall be 50 feet
 - [2] The maximum single tenant commercial area shall be 20,000 square feet.
 - [3] The minimum residential unit size shall be 800 square feet.
 - (e) Setbacks
 - [1] Setbacks shall be regulated by building type. Buildings with property lines adjoining a residential zone or a residential use shall have a vertical setback of 10 feet at every three stories along the property line..
 - (f) Building Height
 - [1] The minimum building height shall be regulated by building type
 - [2] The maximum building height shall be as indicated in the District Regulating Map 3 – Building Heights
 - [3] Vertical Setback From Public Street shall be a minimum of 5 feet for after the fourth story
 - (g) Coverage Requirements
 - [1] Maximum Building Coverage shall not exceed 85 percent
 - [2] Maximum Impervious Converge shall not exceed 90 percent

(h) Permitted Building Types

Only the following Building Types are permitted

- [1] Liner Building
- [2] Courtyard Building; only east of Orchard Street
- [3] Multi-Family Building; only west of Orchard Street
- [4] Corner Building
- [5] Townhouse ; only west of Orchard Street
- [6] Civic Building
- [7] Commercial Block

(5) Allowable Building Types

The allowable building types are defined by Downtown District.

Any existing building within the D2 Blackwell Street Historic District shall be considered a conforming structure. In case of new construction or demolition the regulations of D3 East Blackwell Business District shall apply.

Building Types By Downtown District Summary Table				
	D1 Station Area District	D2 Blackwell Street Historic District	D3 East Blackwell Business District	D4 South Downtown District
Commercial Block (CO)	YES	See Note Below	YES	YES
Liner Building (LB)	YES		NO	YES
Townhouse (TH)	YES		NO	YES*
Courtyard Building (CY)	NO		NO	YES**
Corner Building (CR)	NO		YES	YES
Civic Building (CI)	YES		YES	YES
Multi-Family Building (MF)	YES		NO	YES*
<p>* Only permitted West of Orchard Street</p> <p>** Only permitted East of Orchard Street</p> <p>Note: Any existing building within the D2 Blackwell Street Historic District shall be considered a conforming structure. In case of new construction or demolition the regulations of D3 East Blackwell Business District shall apply.</p>				

Table 236-17.1- 5

(6) Permitted Uses By Building Type

The permitted uses within Downtown Districts are defined by Building Type. Table 236-17.1- 3 summarizes the permitted uses:

Permitted Uses By Building Type Summary table											
Building Types	Permitted Uses										
	Dwelling Unit	Hotel	Business/Professional Office	Bank	Retail Sales and Service	Drive-Through Uses	Restaurant	Theater	Music & Dance Studios, And Fitness Centers	Club /Fraternal Organization	Civic Use/Community Center
Commercial Block (CO)	Pu	P	Pu	P	P	N	P	P	Pu	Pu	P
Liner Building (LB)	Pu	Pu	P	P	P	N	P	P	P	Pu	P
Townhouse (TH)	P	N	N	N	N	N	N	N	N	N	N
Courtyard Building (CY)	Pu	Pu	P	P	P	N	P	P	P	Pu	P
Corner Building (CR)	Pu	P	Pu	P	P	N	P	P	Pu	Pu	P
Civic Building (CI)	N	N	N	N	N	N	N	N	P	P	P
Multi-Family Building (MF)	P	N	N	N	N	N	N	N	N	N	N

All Existing Buildings	Pu	P	Pu	P	P	N	P	P	Pu	Pu	P
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P	Permitted (All Floors) Principal Use – Use may be provided on all floors in this Building Type
Pu	Permitted (Upper Floors) Principal Use – Use may only be provided on the upper floors of this Building Type
N	Not Permitted – Use is prohibited in this Building Type

Table 236-17.1- 6

E. Building Types

(1) Commercial Block (CO)

The Commercial Block is the most predominant building type found on Blackwell Street. It is typically a mixed-use building with retail uses at the street level and residential uses in the upper floors. These buildings are generally built up to the street without any side yard.

The three predominant variants of the Commercial Block, among existing buildings in Downtown are the 25-30 feet wide building, 50-60 feet wide building, and 100-125 feet wide building.

(a) Setback

- [1] The Build-To-Line distance shall be zero.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 5 feet shall be permitted for pedestrian access only.
- [3] There shall be no minimum side rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The minimum number of stories shall be 2.
- [2] The minimum building height shall be 30 feet.
- [3] The minimum height of the ground floor shall be 12 feet
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.

(c) Building Size

- [1] The maximum width of a building shall be 90 percent of the Lot Width, but not to exceed a maximum of 125 feet.
- [2] The minimum depth of ground floor commercial space shall be 40 feet.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.

(e) Maximum. Building Encroachment

- [1] The Maximum. Building Encroachment for a signature building entrance shall be 5 feet.
- [2] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.
- [3] The Maximum. Building Encroachment for an awning shall be 5 feet.

(f) Additional Regulations

- [1] The Upper story storage, internal circulation areas, and staircases shall not be visible from the public street.
- [2] Upper story residential and office entrance lobbies are permitted at the street level.
- [3] Pitched and mansard roofs are not permitted.
- [4] Continuous "glass walls" are not permitted.
- [5] Upper story balconies are not permitted on facades visible from a public street.
- [6] The height of a parking structure shall not exceed the height of principal building.

(2) Liner Buildings (LB)

A liner building is a mixed-use structure, which is generally parallel to the street and designed to enclose a functional core such as a parking garage, and shield the visibility of such structures from a public street, plaza or park. A liner building may include commercial, office, and/or residential uses. Entrances to liner buildings are provided in the form of a common lobby at the ground floor for access to upper story residential and office spaces and as private front doors to access ground-level unit and street-level entrances to retail spaces. If the liner building encloses a parking structure, the rooftop space of the parking structure may be designed as a green roof, which would serve as residential amenity.

(a) Setback

- [1] The Build-To-Line distance shall be zero. A Build-To-Line distance of 10 feet shall be permitted when Terrace or Stoop frontages are provided.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 25 feet shall be permitted for access only.
- [3] There shall be no minimum side rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The minimum number of stories shall be 3.
- [2] The minimum building height shall be 40 feet.
- [3] The minimum height of the ground floor shall be 12 feet.
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.
- [5] The maximum height of a Corner Architectural Element shall be 20 feet.

(c) Building Size

- [1] The maximum width of a building shall be 250 feet.
- [2] The minimum depth of a building shall be 40 feet.
- [3] The Building Frontage at the Build-To-Line shall not exceed 90 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.
- [2] Structured parking may be under habitable space or a green roof.

(e) Maximum. Building Encroachment

- [1] The Maximum. Building Encroachment for a signature building entrance shall be 5 feet.
- [2] The Maximum. Building Encroachment for an upper story balcony shall be 4 feet.
- [3] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.
- [4] The Maximum. Building Encroachment for an awning shall be 5 feet.

(f) Additional Regulations

- [1] The Upper story storage, internal circulation areas, and staircases shall not be visible from the public street.
- [2] Upper story residential and office entrance lobbies are permitted at the street level.

- [3] Rooftop of parking structures may be occupied by a green roof or habitable space.
- [4] Pitched roofs, except mansard, are not permitted for buildings taller than 4 stories.
- [5] Internal access to parking from the mixed-use liner building may be provided at every floor.
- [6] The maximum footprint area of corner architectural elements shall be 300 square feet.

(3) Townhouse (TH)

A townhouse is one in a series of one-family dwelling units that are attached vertically by common fire and sound-resistant walls. Parking for a townhouse may be provided within the unit and accessed from a rear alley; or as a common surface lot or parking structure within the townhouse development.

(a) Setback

- [1] The Build-To-Line distance shall be no more than 10 feet.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 25 feet shall be permitted for access and landscaping only.
- [3] There shall be no minimum side rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The minimum number of stories shall be 3.
- [2] The minimum building height shall be 40 feet.
- [3] There shall be no minimum height of the ground floor.
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.

(c) Building Size

- [1] The minimum width of a building shall be 20 feet. The maximum width of a building shall be 30 feet.
- [2] The minimum depth of a building shall be 40 feet.
- [3] The Building Frontage at the Build-To-Line may equal 100 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.
- [2] Structured parking may be under habitable space or a green roof.

(e) Maximum Building Encroachment

- [1] The Maximum Building Encroachment for an upper story balcony shall be 4 feet.
- [2] The Maximum Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.

(f) Additional Regulations

- [1] The Maximum eave to roofline height of a mansard roof shall be 5 feet.
- [2] The Roofline of a pitched roof shall be parallel to the street.
- [3] Continuous "glass walls" are not permitted.
- [4] Townhouse buildings may be provided as a liner development for a parking structure on blocks west of Orchard Street. In such a case, the height of a parking structure may not exceed the height of the townhouse building.

[5] No more than 6 townhouse buildings may be attached by common walls.

(4) Courtyard Building (CY)

A courtyard building, as permitted by this Code, is a mixed-use residential building with dwelling units arranged around an internal courtyard. The internal courtyard is a residential park area/green roof built on top of an embedded or underground parking deck, and is designed to be an extension of the Crescent Field.

This building type is permitted only east of Orchard Street, and the internal courtyards within these buildings may only front onto Crescent Field.

(a) Setback

[1] The Build-To-Line distance shall be zero. A Build-To-Line distance of 10 feet shall be permitted when Terrace or Stoop frontages are provided.

[2] Side yard setback distance shall be a maximum of 10 feet.

[3] There shall be no minimum side rear setback distance.

(b) Building Height

[1] The minimum number of stories shall be 3.

[2] The minimum building height shall be 40 feet.

[3] The minimum height of the ground floor shall be 12 feet.

[4] The maximum height of parking structures shall be 25 feet.

[5] The maximum height of a Corner Architectural Element shall be 20 feet.

(c) Building Size

[1] The maximum width of a building shall be 250 feet.

[2] The minimum depth of a building shall be 100 feet.

[3] The maximum depth of a retail or residential space shall be 40 feet.

[4] The Building Frontage at the Build-To-Line shall not exceed 90 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

[1] Structured parking may be under habitable space or a green roof.

(e) Maximum. Building Encroachment

[1] The Maximum. Building Encroachment for a signature building entrance shall be 5 feet.

[2] The Maximum. Building Encroachment for an upper story balcony shall be 4 feet.

[3] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.

[4] The Maximum. Building Encroachment for an awning shall be 5 feet.

(f) Additional Regulations

[1] A Courtyard Buildings is only permitted east of Orchard Street.

[2] Street level entrance lobbies for upper story office and residential uses are permitted.

[3] The Maximum footprint area of corner architectural elements shall be 300 square feet.

[4] Courtyards shall be well lit and landscaped.

[5] Courtyards shall only front on Crescent Field.

- [6] The minimum building height on the Crescent Field frontage may be reduced to 2 stories and 25 feet
- [7] Ground level building frontage on Crescent Field shall be occupied by residential amenities, restaurants, cafes, or other types of assembly uses such as bookstores with outdoor seating.

(5) Corner Building (CR)

This is a variant of the Commercial Block designed to create signature buildings on corner lots at prominent intersections. This building type is allowed a greater height and a larger building footprint than the Commercial Block. It is typically a mixed-use building with retail uses at the street level and residential uses in the upper floors.

(a) Setback

- [1] The Build-To-Line distance shall be zero.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 15 feet shall be permitted for access only.
- [3] There shall be no minimum rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The minimum number of stories shall be 2.
- [2] The minimum building height shall be 35 feet.
- [3] The minimum height of the ground floor shall be 12 feet
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.
- [5] The maximum height of a Corner Architectural Element shall be 20 feet.

(c) Building Size

- [1] The minimum width of a building shall be 50 feet. The maximum width of a building shall be 100 feet.
- [2] The minimum depth of a building shall be 40 feet. The maximum depth of a building shall be 100 feet.
- [3] The Building Frontage at the Build-To-Line may equal 100 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.
- [2] Structured parking may be under habitable space.
- [3] Shared parking lots with other buildings are permitted.

(e) Maximum. Building Encroachment

- [1] The Maximum. Building Encroachment for a signature building entrance or theater marquee shall be 5 feet.
- [2] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.
- [3] The Maximum. Building Encroachment for an awning shall be 5 feet.

(f) Additional Regulations

- [1] The Upper story storage, internal circulation areas, and staircases shall not be visible from the public street.
- [2] The height of this building type may be 10% greater than allowed in the Height Regulating Map.
- [3] Continuous “glass walls” are not permitted.
- [4] The Maximum footprint area of corner architectural elements shall be 300 square feet.
- [5] Mezzanine floors are permitted with a minimum ground floor height of 18 feet.
- [6] Upper story balconies are not permitted on facades visible from a public street.
- [7] The height of a parking structure may not exceed the height of principal building.

(6) Civic Building (CI)

This is a building type designed to accommodate public and quasi-public uses. Civic Buildings are intended to accommodate community service uses such as a place of worship, a library, a community hall, a historical (or other) society building, etc. These buildings are intended to become identifiable landmarks within the Town, and hence the quality and style of architecture shall be of great importance.

(a) Setback

- [1] The maximum Build-To-Line distance shall be 25 feet.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 15 feet shall be permitted for access only.
- [3] There shall be no minimum rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The maximum number of stories shall be 3.
- [2] The minimum building height shall be 18 feet. The maximum building height shall be 40 feet.
- [3] The minimum height of the ground floor shall be 14 feet.
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.
- [5] The maximum height of a Corner Architectural Element shall be 20 feet.

(c) Building Size

- [1] The maximum width of a building shall be 100 feet.
- [2] The maximum depth of a building shall be 100 feet.
- [3] The Building Frontage at the Build-To-Line shall not exceed 80 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.
- [2] Structured parking may be under habitable space.
- [3] Shared parking lots with other buildings are permitted.

(e) Maximum. Building Encroachment

- [1] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.
- [2] The Maximum. Building Encroachment for an awning shall be 5 feet.

(f) Additional Regulations

- [1] The scale of entrance shall mimic the proportions of some of the existing public building such as the Town Hall building.
- [2] High quality building materials shall be used, and the scale of architectural elements shall be proportioned to create an iconic building.
- [3] The Maximum footprint area of corner architectural elements shall be 300 square feet.

(7) Multi-Family Building (MF)

This is a multi-family residential building type designed to provide housing opportunities within the Downtown and close to mass transit. The street frontages of these buildings shall be characterized by quality landscaping along the street edge and multiple street-level entries to create opportunities for social interaction.

(a) Setback

- [1] The Build-To-Line distance shall be a minimum of 3 feet and a maximum of 15 feet.
- [2] Side yard setback distance shall be zero. A side yard setback of no more than 25 feet shall be permitted for access and landscaping only.
- [3] There shall be no minimum side rear setback distance.
- [4] The setback distance for surface parking shall be 5 feet.

(b) Building Height

- [1] The minimum number of stories shall be 3.
- [2] The minimum building height shall be 40 feet.
- [3] There shall be no minimum height of the ground floor.
- [4] The maximum height of parking structures shall be equivalent to the maximum permitted building height.
- [5] The maximum height of a corner architectural element shall be 20 feet.

(c) Building Size

- [1] The maximum width of a building shall be 150 feet.
- [2] The minimum depth of a building shall be 70 feet. The maximum depth of a building shall be 100 feet.
- [3] The Building Frontage at the Build-To-Line shall not exceed 90 percent of the Lot Frontage.

(d) Permitted Configuration of Parking

- [1] Both structured and surface parking shall only be permitted in the rear of the building.
- [2] Structured parking may be under habitable space or a green roof.

(e) Maximum. Building Encroachment

- [1] The Maximum. Building Encroachment for a signature building entrance shall be 5 feet.

[2] The Maximum. Building Encroachment for an upper story balcony shall be 4 feet.

[3] The Maximum. Building Encroachment for a cornice, signage or other architectural element shall be 2 feet.

(f) Additional Regulations

[1] Pitched roofs are not permitted for buildings taller than 4 stories.

[2] The Maximum distance between street level building entries shall be 35 feet.

[3] Continuous “glass walls” are not permitted.

[4] The build-to-line distance shall be extensively landscaped to create a useable semi-public space.

F. Frontage Types

(1) Allowable Frontage Types

Street-level building frontages are closely related to the Building Type, the building use at the street level, and the desired characteristics of a specific Downtown District. Hence, the allowable frontage types are provided as a combination of these three parameters. The permitted frontage types are mutually exclusive - no two frontage types can be combined. Table 236-17.1- 7 summarizes the allowable Frontage Types by Downtown District, Building Type, and building use at the street level:

Allowable Frontage Types Summary Table															
Frontage Types	Downtown District			Building Type							Street-Level Building Use				
	D1 Station Area Mixed Use District	D3 East Blackwell Business District	D4 South Downtown District	Commercial Block (CO)	Liner Building (LB)	Townhouse (TH)	Courtyard Building (CY)	Corner Building (CR)	Civic Building (CI)	Multi-Family Building (MF)	Retail Sales and Service/ Bank	Restaurant	Office	Dwelling Unit	Civic
Shopfront & Awning	Y	Y	Y	Y	Y	N	Y	Y	N	N	Y	Y	Y	N	N
Stoop	Y	N	Y	N	Y	Y	Y	N	N	Y	N	N	Y	Y	N
Terrace	Y	N	Y	N	Y	N	Y	N	Y	N	N	Y	Y	N	Y
Arcade	Y	N	Y	Y	Y	N	Y	N	Y	N	Y	Y	Y	N	Y
Y	Frontage Type Permitted														
N	Frontage Type Not Permitted														
<p>Note: As an example of identifying permitted Frontage Types, civic uses at the street level are permitted in the D3 District, but the frontage may only be a terrace, and not an arcade or a gallery. However, if the same civic building use is located within D1 or D4 Districts, terrace and arcade frontage types are permitted.</p>															

Table 236-17.1- 8

(2) Shopfront & Awning

(a) Description

A building frontage wherein the façade is aligned close to the frontage line with the building entrance at sidewalk grade.

It has a substantial glazing on the sidewalk level and may have an awning that may encroach on to the sidewalk.

(b) Height of Finished First Floor from Sidewalk

[1] The first floor shall be no more than 2 feet above the sidewalk.

(c) Clearance Height of All Projections from Finished First Floor (awnings, etc.)

[1] The Clearance Height of All Projections from Finished First Floor (awnings, etc.) shall be a minimum of 8 feet and a maximum of 12 feet.

(d) Depth of Awning

[1] Awnings shall have a minimum depth of 3 feet and a maximum depth of 5 feet.

(e) Width of Frontage

[1] The maximum width of an awning shall be 12 feet.

[2] The maximum width of display windows shall be 12 feet.

(f) Landscaping

[1] Planters and street trees are permitted landscaping along shopfronts.

(g) Building Interface

[1] The maximum distance between doors shall be 35 feet.

[2] Window types shall be fixed or display type.

[3] The minimum glazing shall be 60 percent.

(3) Stoop

(a) Description

A building frontage wherein the building entrance is setback from the edge of the sidewalk, and the first story is sufficiently elevated from the sidewalk to secure privacy for the windows.

The stairs of a stoop shall be perpendicular to the sidewalk.

Stoops may be roofed or unroofed but may not be screened or otherwise enclosed.

(b) Height of Finished First Floor from Sidewalk

[1] The first floor shall be a minimum of 2 feet above the sidewalk.

[2] The first floor shall be no more than 4 feet above the sidewalk.

(c) Clearance Height of All Projections from Finished First Floor (awnings, etc.)

[3] There is no minimum or maximum clearance height of all projections from the finished first floor.

(d) Depth of Frontage

[1] The minimum depth of a stoop shall be 4 feet.

[2] The maximum depth of a stoop shall be to the Build-To-Line.

(e) Width of Frontage

[1] The maximum width of a stoop shall be 8 feet.

[2] The minimum width of a stoop shall be 4 feet.

(f) Landscaping

[1] Street trees and foundation plantings are permitted landscaping.

(g) Building Interface

[1] The maximum distance between doors shall be 35 feet.

[2] Window types shall be fixed residential type.

[3] The minimum glazing shall be 25 percent.

(2) Terrace

(a) Description

A building frontage wherein the façade is setback from the frontage line by an elevated terrace. For street level commercial uses such as restaurants, the terrace is suitable for conversion to outdoor cafes. In civic uses, a terrace can function as a strong base for the building, and also as an outdoor waiting/entrance area.

A terrace shall be paved, and only temporary outdoor furniture and landscaping may be provided on a terrace.

(b) Height of Finished First Floor and Terrace from Sidewalk

[4] The first floor and terrace shall be a minimum of 1 foot above the sidewalk.

[5] The first floor and terrace shall be no more than 4 feet above the sidewalk.

(c) Clearance Height of All Projections from Finished First Floor (awnings, umbrellas, etc.)

[6] The minimum clearance height of all projections from the finished first floor shall be 8 feet.

[7] The maximum clearance height of all projections from the finished first floor shall be 12 feet.

(d) Depth of Frontage

[1] The minimum depth of a terrace shall be 6 feet.

[2] The maximum depth of a terrace shall be to the Build-To-Line.

(e) Width of Frontage

[3] The minimum width of a terrace shall be 15 feet.

[4] The maximum width of a commercial window shall be 12 feet.

(f) Landscaping

[1] Planters and street trees are permitted landscaping.

(g) Building Interface

[1] The maximum distance between doors shall be 35 feet.

[2] Window types for commercial uses shall be fixed/vertical shutter.

[3] The minimum glazing for commercial uses shall be 60 percent.

(3) Arcade

(a) Description

A building frontage wherein the first floor (or the first two floors) façade is setback from the frontage line, and a permanent roof supported by a series of arches on columns or piers (or a colonnade) extends up to the frontage line. The upper floors of the building are built up to the frontage line.

A double story (first and second floors) arcade is permitted.

(b) Height of Finished First Floor and Terrace from Sidewalk

[1] The first floor shall be no more than 2 feet above the sidewalk.

(c) Clearance Height of Arcade from Finished First Floor

[1] The minimum clearance height of arcade from the finished first floor shall be equal to the height of the first floor.

[2] The maximum clearance height of arcade from the finished first floor shall be equal to the height of the first two floors.

(d) Depth of Frontage

[1] The minimum depth of arcade shall be 10 feet.

[2] The maximum depth of arcade shall be 15 feet.

(e) Width of Frontage

[1] The maximum distance between columns shall be 35 feet.

(f) Landscaping

[1] Planters and street trees are permitted landscaping.

(g) Building Interface

[1] The maximum distance between doors shall be 35 feet.

[2] Window types for commercial uses shall be fixed/vertical shutter.

[3] The minimum glazing for commercial uses shall be 60 percent.

G. Street Type Specifications

This section of the Code for Downtown Districts provides design specifications for public streets. Streets in the Downtown are classified as Blackwell Street, Avenue 'A' and Avenue 'B' (Street Types Classification Map). In addition to these three street types, Non Vehicular Ways are provided for mid-block connections and other non-vehicular pathways.

The design standards ensure that the streetscape improvements on these streets are coordinated, and a uniform character of the Downtown Districts is maintained.

(1) Blackwell Street

Blackwell Street represents the Town’s downtown commercial area and contains an assortment of unique shopping venues. It has a mixed-use environment that can be enhanced by pedestrian and streetscape improvements.

The width of the right-of-way throughout this stretch of Blackwell Street is 75 feet. The street profile for Blackwell Street shall generally consist of sidewalks on both sides of the street, two 8 feet wide parallel parking aisles, and two travel lanes. Bulb-outs shall be provided at intersections that are pedestrian intensive.

Blackwell Street – Specifications	
Right-of-Way Width	75'
Pavement Width	45'
Vehicular Movement	Two-way
Number of Traffic Lanes	2
Number of Parallel Parking Lanes	2 parallel parking lanes, 8' wide (one on each side)
Curb Radius	10'
Pedestrian Provision Type	Sidewalk, 13' wide minimum (including curb)
Bicycle Provision Type	Within shared vehicle right-of-way
Median	None
Landscape Type	Street trees at 30'-50' on center
Lighting	Street lights at 30'-50' on center
Intersection Improvements	Curb ramps; Brick pavement and crosswalks at all intersections with 'Avenue A'

Table 236-17.1- 9

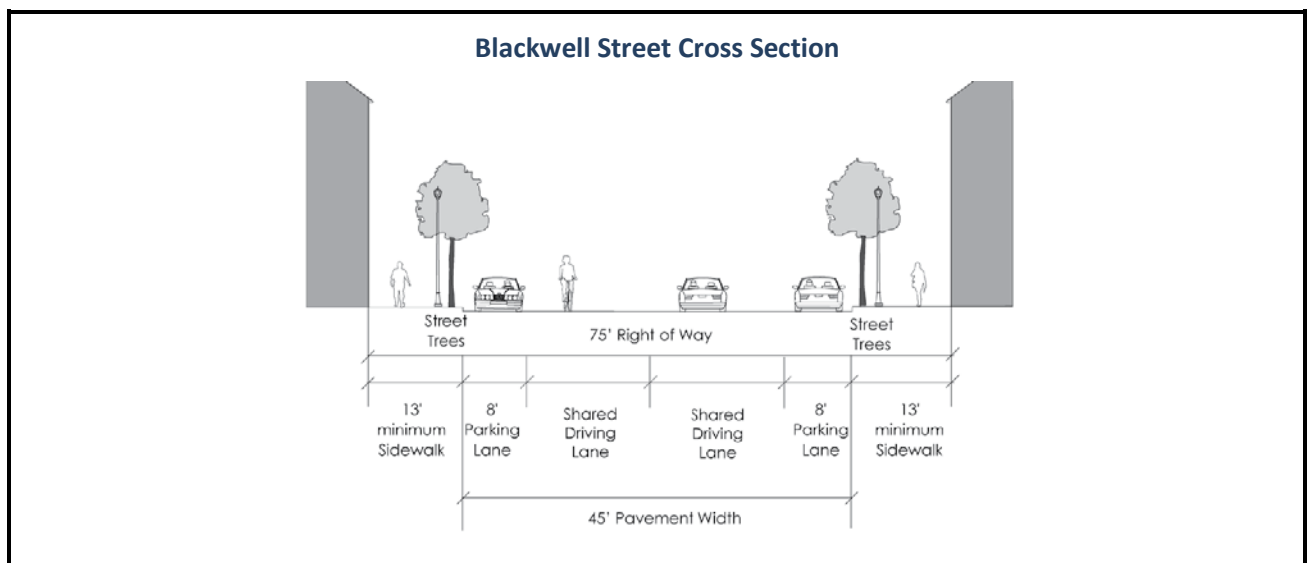


Figure 236-17.1- P

(2) Avenue 'A'

The Avenue 'A' classification includes 60'-66' wide streets that are perpendicular to and north of Blackwell Street. These streets intersect Blackwell Street to the north and traverse over the Rockaway River and the Dover and Rockaway Railroad, while extending north toward Route 46.

The right-of-way width of the streets ranges between 60 and 66 feet. The street profile for these streets shall generally consist of sidewalks on both sides of the street, two 8 feet wide parallel parking aisles, and two travel lanes. Bulb-outs shall be provided at intersections that are pedestrian intensive.

Avenue 'A' – Specifications	
Right-of-Way Width	60' - 66'
Pavement Width	38'
Vehicular Movement	Two-Way, except for N. Morris St., which is one-way.
Number of Traffic Lanes	2
Number of Parallel Parking Lanes	2 parallel parking lanes, 8' wide (one each side)
Curb Radius	10'
Pedestrian Provision Type	Sidewalk, 9' wide minimum (including curb)
Bicycle Provision Type	Within shared vehicle right-of-way
Landscape Type	Street trees at 30'-50' on center
Lighting	Street lights at 30'-50' on center
Intersection Improvements	Curb ramps; Brick paved crosswalks

Table 236-17.1- 10

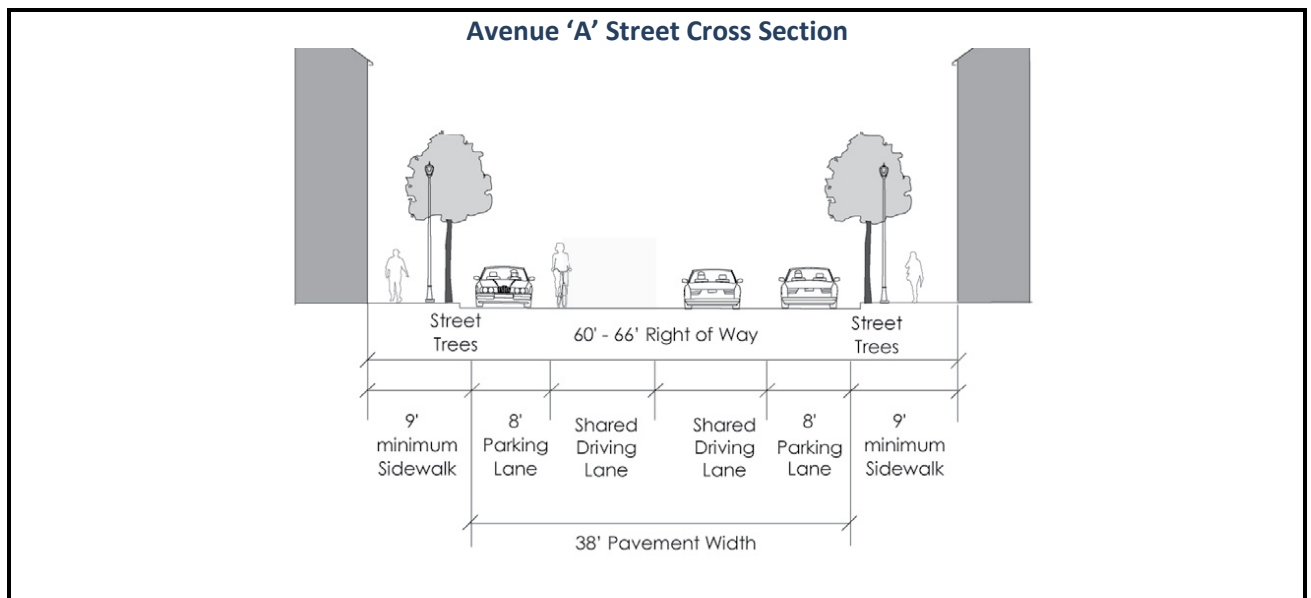


Figure 236-17.1- Q

(3) Avenue 'B'

The Avenue 'B' Classification includes W. Dickerson Street, S. Morris Street, Prospect Street, Thompson Street, and Legion Place. In addition, any newly constructed streets south of the Norfolk Southern Morris & Essex Railroad, shall fall into this street classification.

The right-of-way width of these streets ranges between 40 and 50 feet. The street profile for these streets shall generally consist of sidewalks on both sides of the street, parallel parking aisles, and two travel lanes. Landscaping along the railroad right-of-way on Dickerson Street shall be coordinated with the New Jersey Transit. Bulb-outs shall be provided at intersections that are pedestrian intensive.

Avenue 'B' – Specifications	
Right-of-Way Width	40' - 50' for existing streets 50' minimum for all new streets
Pavement Width	30' - 38'
Vehicular Movement	Two-Way
Number of Traffic Lanes	2
Number of Parallel Parking Lanes	At least 1 side, 8' wide
Curb Radius	10'
Pedestrian Provision Type	Sidewalk, 5' wide minimum (including curb)
Bicycle Provision Type	Within shared vehicle right-of-way
Landscape Type	Street trees at 30'-50' on center
Lighting	Street lights at 30'-50' on center
Intersection Improvements	Curb ramps; Brick paved crosswalks

Table 236-17.1- 11

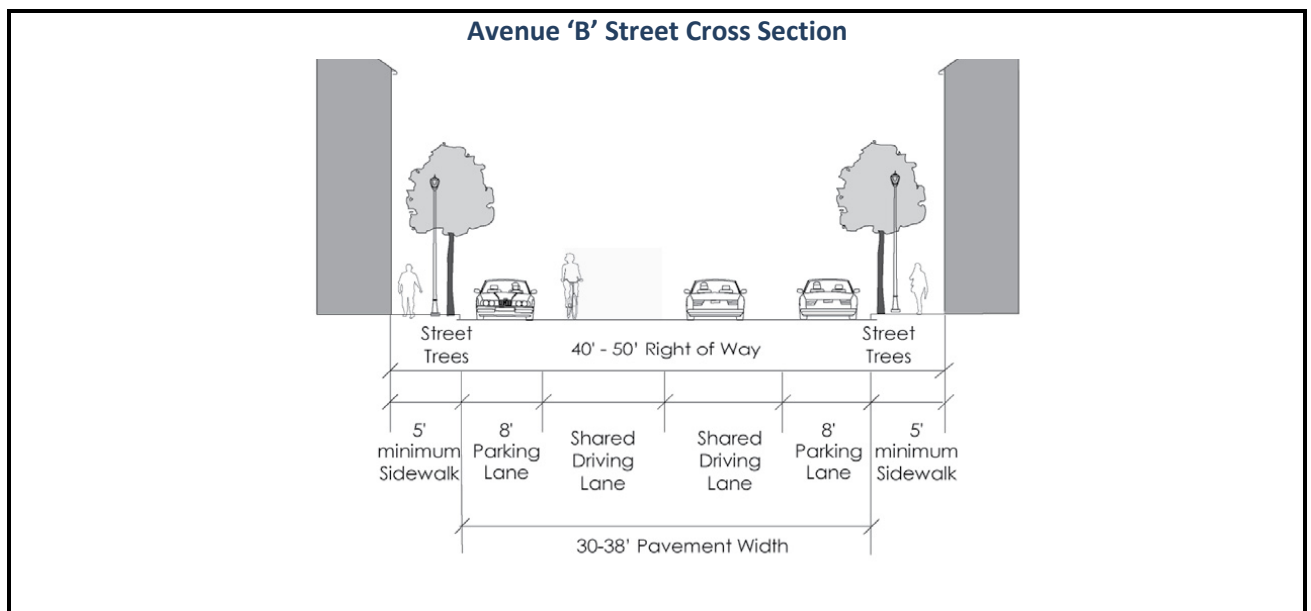


Figure 236-17.1- R

(4) Non Vehicular Ways

The “Non Vehicular Ways” Classification is provided as a template for all non-vehicular pathways and mid-block connections.

The right-of-way width of these non-vehicular pathways shall be no less than 14 feet. The street profile for Non Vehicular Ways shall generally consist of a 2 foot buffer for optional landscaping buffer and street furniture on both sides; and pavement for shared pedestrian and bike travel.

Non Vehicular Ways – Specifications	
Right-of-Way Width	14'
Pavement Width	10'
Vehicular Movement	Two bike/ped lanes
Number of Lanes	2, 5' wide
Number of Parallel Parking Lanes	n/a
Curb Radius	10'
Pedestrian Provision Type	Shared pedestrian and bike travel
Bicycle Provision Type	Shared pedestrian and bike travel
Landscape Type	2' landscaping strip with street trees and Low shrubs
Lighting	Pedestrian scaled lighting at 30'-50' on center
Intersection Improvements	Curb ramps; Brick paved crosswalks

Table 236-17.1- 12

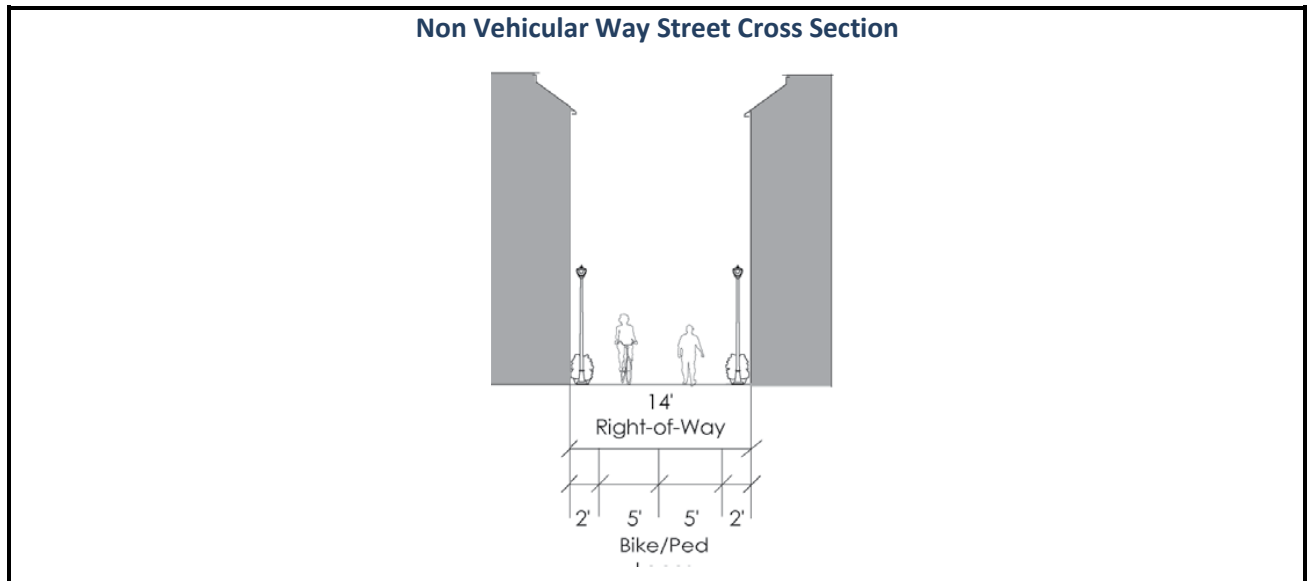


Figure 236-17.1- S

H. General Regulations for Downtown Districts

The following regulations represent a general set of standards that apply to all of the Downtown Districts.

(1) Building Frontage

- (a) For properties fronting on key open space assets (i.e. the Rockaway River and Crescent Field), the required building frontage percentage may be reduced up to 35% provided that the reduced frontage allows open views and/or capitalizes on access to these assets.
- (b) For buildings facing key view sheds (i.e. the Rockaway River and Crescent Field), it is encouraged that restaurants and cafes be provided at the ground level, to enhance the visual appeal and activity.

(2) Public Parking

(a) Parking Garages

Designated locations for parking garages that serve the commuter and public parking needs within the downtown are indicated on the District Regulating Map 2. Liner buildings, articulated facades, art walls or advertising windows shall be provided to integrate the parking garages into the built environment.

(b) Downtown and Commuter Parking

Any redevelopment of existing public surface parking lots within the downtown area must accommodate commuter and downtown parking needs in addition to the parking needs of such redevelopment. Table 236-17.1- 13 shows the number of parking spaces and the figure below indicates the location of the parking lots, as of the date of adoption of this Code. The determination of the actual number of spaces to be accommodated shall be made by the applicant based on a study of the availability and need of commuter and downtown parking at the time of redevelopment. Such determination shall be subject to review and approval by the Planning Board. Town Lot 'A' is purposely not shown since it is reserved exclusively for public parking.

Parking Lot	Existing Number of Spaces
Lot "B" – Crescent Field	350
Lot "C" – Orchard Street & Legion Place	58
Lot "D" – Prospect Street	181
Lot "E" – Sussex /Dickerson East Side	40
Lot "G" – Sussex /Dickerson West Side	45
Lot "H" – Dickerson West of Warren	18

Table 236-17.1- 14

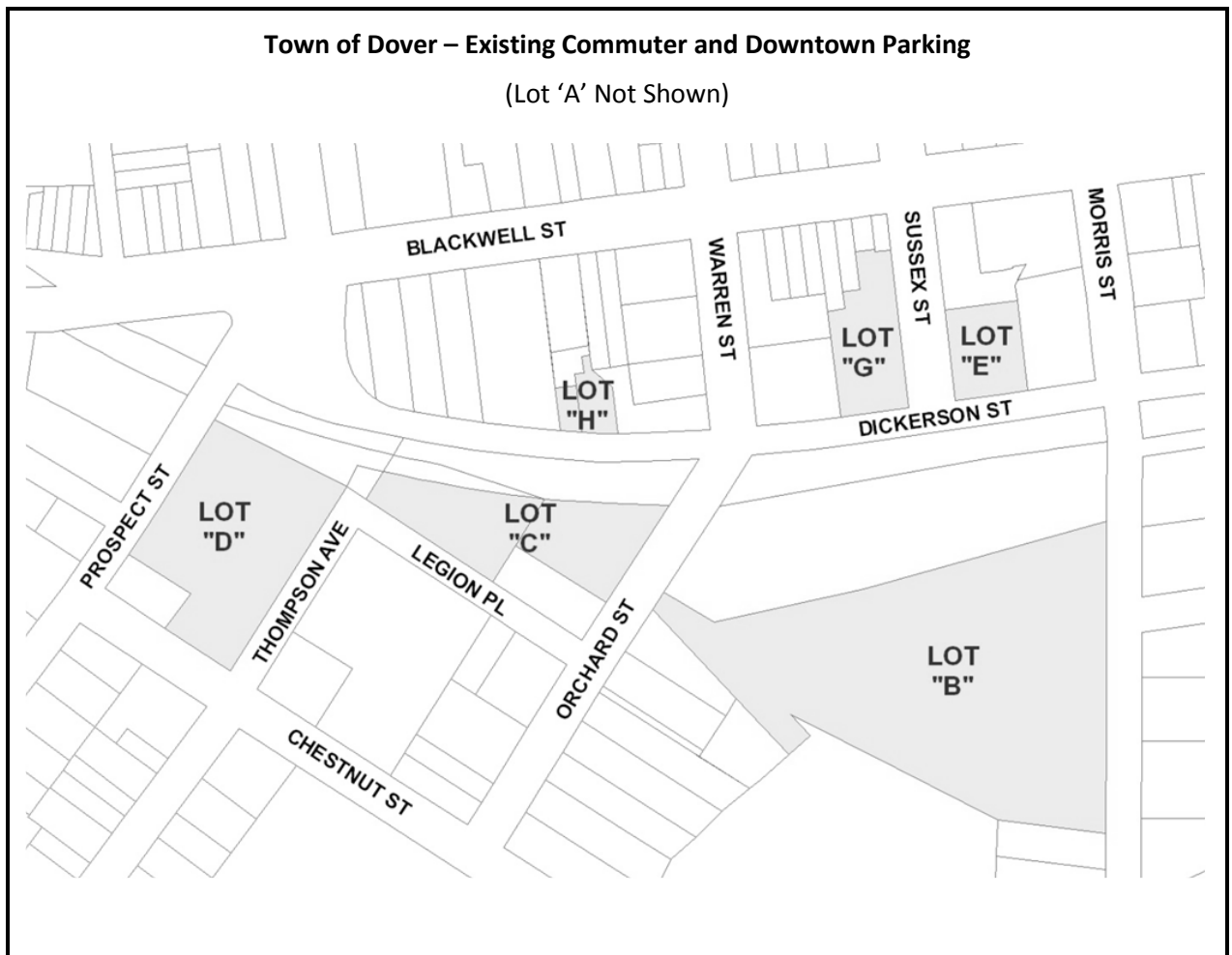


Figure 236-17.1- T

(3) Parking Requirements

(a) Off-Street Parking Provisions

The Downtown Districts are planned as “park-once” districts, wherein people are expected to park in one place and then make stops on foot rather than driving from one destination to another, with preference given to pedestrian movement within the districts. For this reason, substantial reductions are allowed in the off-street parking requirements.

[1] All existing buildings shall be regulated as follows:

- [a] All residential uses shall be required to provide off-street parking in accordance with the parking ratios for new construction (see below).
- [b] For redevelopment of a lot, for which the cost of redevelopment exceeds 50% of the value of existing improvements, as determined by the Tax Assessor, and for which site plan review is required, off-street parking shall be in accordance with the parking ratios for new construction (see below).
- [c] For all other permitted nonresidential uses, there shall be no off-street parking requirements.

[2] The following ratios shall apply to determine the off-street parking space requirements for all new construction:

[a] Residential:

1.3 spaces per unit average. Further broken down by unit type:

[i] 1.0/unit – 1 bedroom

[ii] 1.5/unit – 2 bedroom

[iii] 1.75/unit – 3 bedroom

When an applicant does not specify the number of bedrooms per unit, the 1.3 spaces per unit average shall apply.

[b] Office:

[i] 1 space/1,000 square feet

[ii] 1.5 spaces/1,000 square feet for buildings over 50,000 square feet

[c] Retail:

[i] All retail, including restaurants shall be exempt from the parking standard.

[ii] Retail is meant to utilize on-street parking as well as spaces unoccupied within the commuter or public parking lots.

[3] Any deviation from the off-street parking requirements stated above will require a 'c' variance.

(b) Shared Use of Off-Street Parking Areas

Parking areas of new developments shall be interconnected across lot lines to facilitate shared use of parking and eliminate or minimize driveway cuts. To ensure the effective use of these connections, the first property to develop shall be required to make an irrevocable offer of cross-access to the adjacent parcel (prior to issuance of approval by any land development board), and must design and build the parking lot to accommodate cross-access.

Individual property owners shall control all rights to the use of their own parking spaces, but may choose to allow wider use of these spaces through reciprocal arrangements with the Town of Dover Parking Utility.

All new development that relies on shared parking must demonstrate the adequacy of parking based on established standards/methodology such as those prescribed in "Shared Parking", authored by Mary S. Smith et al. and published by the Urban Land Institute (ULI). This book contains the information needed to accurately estimate parking requirements for a mixed-use setting where parking is shared among the uses.

Any deviation from these requirements shall require a waiver.

(c) Driveway Connections

For existing buildings within D2 and D3 Districts, relocation of all existing access driveways to parking and loading areas from Blackwell Street is encouraged. New driveways shall connect only to 'Avenue A' Street Type, except where this would prohibit all reasonable access to a property.

The following shall apply to all new development:

[1] To the extent possible, driveways shall be shared between adjoining developments.

[2] Where feasible, driveways shall front on streets that are less pedestrian intensive, on existing easements, or on alleys.

[3] Driveways and other vehicular access shall not be provided from or adjacent to public plazas.

[4] Any deviation from these requirements shall require a waiver.

(d) Parking Design

Off-street parking is encouraged to be as inconspicuous as possible and to incorporate landscaping and screening to the greatest extent possible to minimize its physical and visual impact. Off-street parking and loading areas shall be coordinated with the public street system serving the Area in order to avoid conflicts with through-traffic, obstruction to pedestrian walks and vehicular thoroughfares. Shared parking among mixed uses is encouraged.

[1] All parking and loading areas shall be screened from view with walls, shrubs, and/or trees.

[2] All ninety (90) degree parking spaces that are long-term in usage shall be a minimum of eight and one-half (8.5) feet in width and eighteen (18) feet in depth. Aisles shall be a minimum of twenty-two (22) feet in width. Ten (10) percent of required parking spaces may be provided as compact spaces measuring eight (8) feet in width and fifteen (15) feet in depth.

[3] All parking structures shall be designed using compatible or complementary materials to the principal buildings so that they blend in architecturally. All voids in the structures shall be architecturally screened, so that lights and vehicles are not individually visible.

[4] No blank walls of parking structures shall front the streetscape. All facades shall provide pedestrian interest at the street level either through retail uses and/or architectural details.

[5] All permanent parking spaces shall be in structures or screened from street view.

[6] Surface parking shall include decorative street lighting to be compatible with public lighting.

[7] All pedestrian pathways across and along parking areas shall be well lit with pedestrian scaled lighting fixtures.

[8] Vehicular access to internal parking structures shall be designed so as not to negatively impact upon major pedestrian routes. If necessary, provide "fish eye" mirrors or alarms to manage the interaction between pedestrians and vehicles.

[9] Parking signage shall be consistent with signage standards of this code.

[10] Residential garages facing the public right-of-way, and parking within front yard setbacks are prohibited.

[11] The potential conflicts between truck delivery, vehicular traffic, and pedestrian circulation shall be considered when designing service entries, roadways, walkways, and pedestrian entrances.

[12] To the extent possible, service entrances and loading areas between adjacent buildings shall be consolidated. Such service entrances shall be separated from walkways and pedestrian entrances.

[13] Landscaping, fencing and/or low walls shall be provided to screen trash receptacles and dumpsters in loading areas.

[14] Any deviation from these requirements shall require a waiver.

(4) Open Space and Buffers

- (a) Portions of properties adjacent to Crescent Field. The courtyard buildings shall have their private open space oriented towards Crescent field to maximize the perception of open space and minimize the building mass in proximity to Crescent Field. Such private open space should be fully integrated into the design of new development while enhancing surrounding areas.
- (b) Properties adjacent to the Rockaway River and the adjoining Dover Rail R.O.W. shall provide a landscaped buffer consisting of shrubs, and/or trees along this edge.
- (c) Any deviation from these requirements shall require a variance.

(5) Outdoor Uses

- (a) Outdoor service and sale of food is permitted as follows:
 - [1] Tables, umbrellas, and chairs may be placed by restaurants for the use of their customers; no signage is permitted except lettering on umbrellas up to 8 inches in height.
 - [2] Tables and carts shall be placed so that they do not block pedestrian movement along the sidewalk.
 - [3] Tables and chairs shall be moved indoors during non-business hours.
 - [4] A minimum depth of 6 feet of unobstructed sidewalk must remain.
- (b) Sale of merchandise is not permitted other than restaurants.

I. Architectural Standards

(1) Exterior Walls

All exterior walls shall be clad with building materials that are durable and appropriate to the visual environment and climate. Design flexibility and creativity is encouraged using ornamentation from a wide variety of architectural styles but should be complementary to the historic buildings within the downtown.

- (a) The following materials are encouraged for exterior walls, columns, arches, and piers:
 - [1] Natural stone or brick
 - [2] Wood, pressure-treated or naturally decay-resistant species
- (b) Fastenings that are required to dry flood-proof the first story of commercial buildings shall be integrated into the design of principal facades or be visually unobtrusive.
- (c) Building walls shall be consistent in detail and quality on all elevations visible from public streets.
- (d) Trim elements and visible window framing shall be painted or sealed.

(2) Principal Façade Walls

(a) Façade Elements

Principal facades are the facades facing a primary street, plaza, or public park. Being in full public view, they shall be given special architectural treatment.

- [1] All principal facades shall have a prominent cornice and expression line, a working entrance, and windows (except for side-wall facades where entrances are not required).
- [2] Principal facades may not have blank walls (without doors or windows) for more than 15 feet of façade length.
- [3] Expression lines or decorative molding shall be provided for horizontal definition, and shall project at least 3 to 6 inches out from the principal façade.

- [4] Provision of antennas, air conditioning units or other similar equipment on the principal façade walls is prohibited.
- [5] Awnings may not hide or substitute for required features such as expression lines and cornices.
- [6] The foundation walls of stoops must be consistent with the foundation treatment of the building.

(b) Entrances

A primary entrance and views into the first floor of commercial buildings are fundamental to creating an interesting and safe pedestrian environment.

- [1] The primary entrance to all buildings shall front on a public street.
- [2] Corner buildings shall have their primary entrance face on either the intersection or the street of greater importance.

(c) Windows

Every principal facade must contain transparent windows on each story.

[1] Residential windows and doors

- [a] Openings for windows and windowpanes must have a vertical dimension greater than or equal to the horizontal dimension.
- [b] Window openings shall cover at least 30% of the wall area below the expression line;
- [c] Triangular, circular or other unusually shaped windows are not permitted.
- [d] All lintels must extend a minimum of 4 inches beyond the edge of the opening.
- [e] Windowsills shall project a minimum of 2 inches from the building face.
- [f] Glazing shall be set back at least 3 inches from the surface plane of the wall, or set back at least 2 inches when wood frame construction is used.
- [g] If exterior shutters are used, they shall be sized and mounted appropriately to fit their window (with appropriate hardware even if actually non-operable.)
- [h] Glass in windows and doors, whether integrally tinted or with applied film, must transmit at least 50% of visible daylight.
- [i] Glass blocks are not permitted.
- [j] Doors, except garage doors, shall be or appear to be constructed of planks or raised panels (not flush with applied trim.)

[2] Street level retail fenestration

In order to provide clear views inward and to provide natural surveillance of exterior spaces, the first story of every commercial building's principal facade shall have transparent windows meeting the following requirements:

- [a] Window openings shall cover at least 60% of the wall area below the expression line.
- [b] Glazing must be at least 70% transparent.
- [c] The maximum percentage of glass that may be blocked with interior fixtures or paper signs shall be 25%.
- [d] The height of windowsill above the sidewalk shall not be more than 3 feet.
- [e] These windows shall be maintained so that they provide continuous view of interior spaces lit from within. Private interior spaces such as offices may use operable interior blinds for privacy.

[f] Exterior security grates are prohibited. Interior security grates must be as inconspicuous as possible and of open mesh to allow interior visibility from the street.

[g] Glass blocks are not permitted.

[3] Façade Projections

Facade projections add visual interest to buildings. Some projections also provide protection from the sun and rain for those passing by; others provide additional floor space for the building.

[a] Awnings and Canopies

[i] Awnings and canopies shall be provided in accordance with §236-38.1 Signage, Awnings and Canopies in the C-1 and C-3 Districts.

[b] Balconies (where permitted)

[i] Balconies are not permitted in the D2 and D3 Districts.

[ii] No balconies may be located within ten (10) feet of a shared lot line. Glass parapets or railings are prohibited.

[iii] Balconies cannot exceed the following dimensions:

1. Height: minimum clear height of 10 feet from the sidewalk.
2. Length: a maximum of 60% of the upper-story building frontage.
3. Depth: a minimum of 3 feet.

[iv] Balconies may have roofs, but are required to be open and may not be screened or otherwise enclosed.

[c] Bay Windows

[i] Bay windows are only permitted in the Townhouse Building Type.

[ii] Bay windows may not encroach on the public right of way.

[iii] Windows extending from the first story cannot exceed the following dimensions:

1. Depth: 3 feet maximum
2. Height: 10 feet maximum
3. Length: 6 feet maximum

[iv] Bay windows shall have the same details required for principal facades: sills, lintels, cornices, and expression lines.

(3) Roofs

(a) All flat roofs must have their edges along all streets concealed with parapets and trimmed with decorative cornice.

(b) All hip roofs and gable roofs, and any shed roof with a slope of more than 2 inches vertical rise per 12 inches horizontal run, must have a minimum roof overhang of 18 inches.

[1] Exposed rafter ends (or tabs) are encouraged.

[2] Wide overhangs are encouraged and can be supported with decorative brackets.

(c) Small towers, cupolas, and similar architectural elements are encouraged.

(d) Roof penetrations, except stucco or brick chimneys, shall be placed so as not to be easily visible from streets and painted to match the color of the roof

- (e) Dormers are permitted and encouraged on sloped roofs.
- (f) Visibility of skylights must be minimal. Skylight glazing must be flat to the pitch of the roof if the skylight is visible from a primary street, plaza, or public park.
- (g) Roofs shall be constructed of building materials that are durable and appropriate to the local climate and visual environment.

(4) Plazas and Courtyards

New commercial buildings are generally oriented to public sidewalks. This section addresses other public open spaces that also can affect the orientation of commercial buildings.

(a) Plazas

- [1] The width of the plaza cannot exceed 25% of a building's frontage.
- [2] The plaza shall be strictly for pedestrian usage and cannot be used to park vehicles.
- [3] All building walls that surround the plaza must meet the design criteria for principal facades.
- [4] The plaza shall be appropriately landscaped.

(b) Courtyards

The following standards shall apply for interior courtyards designed for public or private usage:

- [1] Courtyards are encouraged to have clear visual linkages between the courtyard and public sidewalks.
- [2] Courtyards are encouraged to provide passive and active recreational amenities for their residents such as play areas and lawn areas.

(c) Pedestrian Passages

Pedestrian pathways, may be provided on private property as mid-block connections; to connect a courtyard to the sidewalk system; to provide walkways to parking lots behind buildings; or to provide additional retail frontage. Such pedestrian pathways shall utilize the specifications for Non Vehicular Ways, detailed in the Street Type Specifications section of this Code.

(5) Signage

Signage shall be provided in accordance with §236-38.1 Signage, Awnings and Canopies in the C-1 and C-3 Districts.

J. Sustainability / "Green Design"

The following regulations apply to all new construction, and concentrate on necessary steps in creating a sustainable, mixed use, pedestrian friendly environment that protects and enhances natural resources while providing individuals and families with safe, healthy and comfortable places to live, work, and recreate.

(1) Energy Efficient Building Design (where applicable)

- (a) Appliances and fixtures must meet U.S. EPA's Energy Star standards. Projects must include Energy Star compliant clothes washers, dishwashers, refrigerators, ceiling fans, ventilation fans (including kitchen and bathroom fans), light fixtures (halls and common areas), and exit signs.
- (b) Project must install at least two of the following Energy Star components: Programmable thermostats; fluorescent or light emitting diode (LED) light fixtures; high-performance windows and doors; and efficient HVAC systems.

(2) Transportation Emissions

Transportation generates approximately 31 percent of total carbon emissions. Facilities are encouraged to increase use of alternative modes such as walking, biking, transit, and car-pooling and thereby reduce vehicle use and carbon emissions.

Where feasible:

- (a) For buildings with institutional, commercial and office uses, provide the following:
 - [1] Secure bike racks and/or storage for at least 5% of building users, located no more than 200 yards from the building entrance
 - [2] Shower and changing facilities in the building, or within 200 yards of a building entrance, for at least 0.5% of building users
 - [3] For multi-family residential buildings, provide secure bike racks and/or storage for at least 15% of building occupants.
- (b) Provide designated preferred parking spaces to carpool users to encourage shared vehicle use and reduce trip generation.
- (c) Provide designated preferred parking spaces to low-emission and fuel-efficient vehicles.

(3) Renewable Energy

Where feasible, solar panels shall be provided on rooftops to capture renewable energy. Such roof top installations shall be screened from view.

(4) Energy Efficient Infrastructure

To the extent feasible:

- (a) Any outdoor lighting fixtures shall use light emitting diode (LED) technology.
- (b) Design or purchase water and wastewater pumps and treatment systems to achieve a 15% annual energy reduction beyond baseline energy use for similar infrastructure.
- (c) Design and implement a district heating and/or cooling system for multiple buildings within a development.

(5) Reflectivity and Heat Island Effects

To the extent feasible

- (a) Use roofing materials having a Solar Reflectance Index (SRI) of 78 for low-sloped roofs (less than 2:12) or 29 for steep-sloped roofs (greater than or equal to 2:12) for a minimum of 75% of the roof surface. The cooler roof (roofing material with a high Solar Reflective Index) reduces the heat transferred into the

building, thereby reducing the amount of air conditioning needed. The Energy Star® program features product specifications for “cool roofs”.

(b) Provide a combination of any of the following strategies for 50 percent of the site’s hardscape:

- [1] Shade – trees or manmade structures
- [2] Paving materials with a SRI of 29 or greater
- [3] Open grid pavement

(6) Landscaping

(a) Where feasible use captured rainwater or other recycled and properly treated non-potable water for landscape irrigation.

(b) Landscaping materials must include only

- [1] Non-invasive species
- [2] Drought tolerant species
- [3] Native or adapted species

(c) Landscaped areas shall be mulched to conserve moisture and prevent water loss from evaporation.

K. Streetscape Standards

The following Streetscape Standards are meant to guide public investment in streetscape improvements, and to assist property owners and developers in designing their own building’s relation to the public realm. These standards specify the allowable planters, trashcans, bike racks, bus stops, lighting, and paving materials.

(1) Street Trees

- (a) Street tree layout will be based upon final engineered streetscape layout.
- (b) Street trees shall be placed on property lines (between properties) so as not to block storefronts.
- (c) Street trees shall be at least 1-3/4 – 2” in trunk caliper measured 6” above the ground, and meet specifications set forth in American Standard for Nursery Stock (ANSI Z60.1-2004).
- (d) No more than 10% of all trees in the entire downtown area or 50% of all trees on a single block may be of the same species.
- (e) Plant materials and irrigation techniques that require less water should be considered.
- (f) Columnar shaped trees are perhaps the best at fitting into the urban environment where narrow sidewalks and awnings restrict the growth of a larger canopy.
- (g) Smaller leafed, spreading forms allow better visibility to building facades and signage. Smaller leaves generally are dispersed by the wind, require less fall cleanup and are less apt to clog storm sewers.

(2) Shade Trees

The shade trees listed in §236-96 Schedule A: Preferred Tree List. Prepared by the Dover Shade Tree Commission are permitted by right. Other trees may be submitted as an alternative if the alternative accomplishes the stated purpose.

(3) Tree Grates

- (a) Tree grates and coordinated tree guards are required for all street trees.
- (b) Tree grates shall be set parallel and flush with the curb line.
- (c) Tree grates shall be made of heavy grade, cast iron or cast aluminum of black finish. Cast iron Model R-8757, 4’ X 4’ Square as produced by Neenah Foundry Company, or equivalent shall be used.

- (d) The center hole of the tree grate shall be expandable to accommodate tree growth. The initial center hole shall be a minimum of 12 inches in diameter.
- (e) Electrical outlets should be provided in the tree grate area.

(4) Planters

- (a) Flowering plants and decorative shrubs should be planted in raised freestanding planters (not in ground at grade level). Seasonal plantings may be used from November 1 - January 10. Annual flowers or ground covers may also be planted in planters.
- (b) Planters shall be "Rosa Planter", 30"x18" or 36"x23", granite finish, as manufactured by Landscape Forms, or equivalent.
- (c) Provisions must be made for ensuring adequate watering and drainage.
- (d) Planters should not block other elements such as streets, signs, meters, or streetlights.
- (e) Planters should be located at corners of intersections, focal points, and other locations where space permits and shall be generally placed at the end of a parking space.

(5) Benches

- (a) Benches shall be "Plainwell Bench", 72" length wooden seat, black color, as manufactured by Landscape Forms, or equivalent.
- (b) Benches located along the curb shall face the sidewalk and be set back from areas of on-street parking to accommodate auto door swing and shall generally be placed at the end of a parking space. Seating should not interfere with plant materials or pedestrian circulation.
- (c) Benches may also be placed adjacent to building walls subject to approval by owner.
- (d) Seating should be secured permanently to paved surfaces for safety and to avoid vandalism.
- (e) Comfortable seating should provide a sense of having protection from behind and something interesting to look at such as shop-fronts or other pedestrians.

(6) Bicycle Racks

- (a) Bicycle racks shall be "Hoop Rack Heavy Duty", as manufactured by Dero Bike Racks, or equivalent shall be used.
- (b) Bicycle racks shall be black in color, and galvanized (electropolished to reduce maintenance).
- (c) Bicycle racks shall be placed near entrances or gathering places. Placement shall be avoided that creates a tripping hazard. If possible, place the racks where the parked bicycles will be visible from inside the adjacent building. Ideally, bicycle parking should be more convenient than automobile parking.
- (d) Bicycle racks shall be securely fastened to the sidewalk.
- (e) Bicycle rack locations shall not interfere with pedestrian circulation or endanger the safety of pedestrians or cyclists; they shall be sensitive to accessible requirements.

(7) Trashcans

- (a) Trash and recycling receptacles, at a minimum, are required at each corner of all intersections. They should be conveniently placed near benches, bus stops and other activity nodes, arranged with other streetscape elements into functional compositions and where feasible, placed at the end of parking spaces. Trashcans may be left freestanding or surface mounted on site.
- (b) The trash receptacles shall be Steelsites Series Model RB-36 with 45-gallon total capacity as manufactured by Victor Stanley, or equivalent.

- (c) Trash and recycling receptacles should be designed in two pieces. The inner container should ensure easy trash pickup and removal and an outer shell should blend aesthetically with the other streetscape elements.

(8) Bus Shelters

- (a) The location of bus stops would be as prescribed by New Jersey Transit, in coordination with the Town of Dover.
- (b) The bus shelters shall be “Kaledioscope”, black color, as manufactured by Landscape Forms, or equivalent.

L. Definitions

ARCADE - is a building frontage wherein the first floor (or the first two floors) façade is setback from the frontage line, and a permanent roof supported by a series of arches on columns or piers (or a colonnade) extends up to the frontage line. The upper floors of the building are built up to the frontage line. A double story (first and second floors) arcade is permitted.

AWNING - means a flexible roof-like cover that extends out from an exterior wall and shields a window, doorway, sidewalk, or other space below from the elements.

BALCONY - means an elevated platform that forms the outdoor area of a habitable space within the building. A balcony may extend beyond the property line.

BAY WINDOW - means a window, which projects beyond the wall of a building to form an alcove within.

Building Encroachment means an entry feature, balcony, architectural element, or awning that extends outward from a building.

BUILD-TO-LINE - build-to line identifies the distance from the street line that the front of all primary structures must be built to in order to create a fairly uniform line of buildings along streets. Where a build-to line is specified as a range (the minimum and maximum distance from the street line), this means that building fronts may fall within that range of distances from the street line. Where there is a range, the front facade does not have to be in a single plane, as long as the front facade remains within the range.



BUILDING FRONTAGE - means the length of a buildings primary façade that faces a street or a public plaza.

BUILDING TYPE - means the principal structure.

CIVIC BUILDING - is a building type in the Downtown Districts that is designed to accommodate public and quasi-public uses. Civic Buildings are intended to accommodate community service uses such as a place of worship, a library, a community hall, an historical (or other) society building, etc. These buildings are not operated primarily for profit or monetary gain.

CIVIC USE - means a public or quasi-public use including but not limited to a house of worship, library, school, municipal use, or public open space.

CLUB OR FRATERNAL ORGANIZATION - is a community organization that is not operated primarily for financial gain, and consists of a group of people formally organized for a common interest, usually service, cultural, religious, or entertainment, with regular meetings, rituals, and formal written membership requirements.

COLONADE - is similar to an arcade except that it is supported by vertical columns without arches.

Commercial Block is a multistory building in the Downtown Districts, with multiple dwelling units or offices in the upper stories and retail uses on the ground floor. Typically, these buildings are separated by common walls on the side lot lines.

CORNER ARCHITECTURAL ELEMENT - is a cupola or a similar structure provided on top of a building or ventilation or aesthetic purposes. Such structures shall not contain habitable spaces.

CORNICE - means a decorative horizontal feature that projects outward near the top of an exterior wall.

CORNER BUILDING - is a building type in the Downtown Districts. It is a variant of the Commercial Block designed to create signature buildings on corner lots at prominent intersections. This building type is allowed a greater height and a larger building footprint than the Commercial Block.

COURTYARD - means an unroofed area surrounded by buildings.

COURTYARD BUILDING - is a building type in the Downtown Districts, in which a mixed-use residential building with dwelling units arranged around an internal courtyard. The internal courtyard is a residential park area/green roof built on top of an embedded underground parking deck,

DORMER - means a projection from a sloping roof that contains a window and its own roof.

EXPRESSION LINE - means a decorative horizontal architectural detail that projects outward from an exterior wall to delineate the top of the story of a building.

FAÇADE - means the exterior face of a building, including but not limited to, walls, window sills, doorways, and such traditional design elements as horizontal lines, cornice, and parapet.

FRONTAGE LINE - means the property line along a public street.

GABLE ROOF - means a ridged roof forming a gable at both ends.

GROUND FLOOR - means the floor of a building at or nearest to ground level.

HIP ROOF - means a roof with pitched ends and sides.

LINTEL - means a structural or merely decorative horizontal member spanning a window opening.

LINER BUILDING - is a mixed-use structure, which is generally parallel to the street and constructed in front of designed to enclose a functional core (e.g. a parking garage) and shield its visibility of from a public street, plaza or park.

LOT FRONTAGE - means the length of a property that is adjacent to any street excluding alleys.

MANSARD ROOF - means a roof having two slopes with the lower slope steeper than the upper, or a single steep slope topped with a flat roof, enclosing the building's top floor. A modern variant is a partial sloped roof that is attached near the top of an exterior wall in place of a traditional cornice or parapet, creating the visual effect of a sloped roof on a flat-roofed building but without enclosing any floor space.

MULTI_FAMILY BUILDING - is a residential building type in the Downtown Districts that is designed to provide housing opportunities within the Downtown and close to mass transit.

PARAPET - means a short vertical extension of a wall that rises above roof level, hiding the roof's edge and any roof-mounted mechanical equipment.

PLAZA - means an unroofed public open space designed for pedestrians that is open to public sidewalks on at least one side.

PRINCIPAL FAÇADE - is a facade facing a primary street, plaza, or public park; and containing the main building entrance.

PUBLIC REALM - is the space between buildings, including publicly owned streets, sidewalks, rights-of-ways, parks and other publicly accessible open spaces, and public and civic buildings and facilities

RETAIL SALES & SERVICE - is a use category that includes retail sales and service establishments excluding drive-thru facilities such as clothing and apparel stores, book stores, sporting goods stores, drug stores, barber shops, beauty salons, cleaners who do not perform cleaning on the premises, stationers stores, jewelry stores, and office supply stores.

SHOPFRONT & AWNING - is a building frontage wherein the façade is aligned close to the frontage line with the building entrance at sidewalk grade. It has a substantial glazing on the sidewalk level and may have an awning that may encroach on to the sidewalk.

SIGNATURE BUILDING ENTRANCE - means a theater marquee or a similar cantilevered projection provided at the street level to mark a residential or hotel entrance. Such projection shall have a clear height of at least 10 feet from the sidewalk level.

SILL - means a horizontal member beneath a window opening, constructed of wood, stone, concrete, or similar material.

STOOP - is a building frontage wherein the building entrance is setback from the edge of the sidewalk, and the first story is sufficiently elevated from the sidewalk to secure privacy for the windows. The stairs of a stoop shall be perpendicular to the sidewalk.

STREETSCAPE - means a representation of a street and its surrounding environment.

TERRACE - is a building frontage wherein the façade is setback from the frontage line by an elevated terrace. For street level commercial uses such as restaurants, the terrace is suitable for conversion to outdoor cafes. In civic uses, a terrace can function as a strong base for the building, and also as an outdoor waiting/entrance area.

TOWNHOUSE BUILDING - is one in a series of one-family dwelling units that are attached together by common fire and sound-resistant walls. Parking for a townhouse may be provided within the unit and accessed from a rear alley; or as a common surface lot or parking structure within the townhouse development.

VERTICAL SETBACK - means a building height setback along its frontage on a public street. Vertical setbacks are used to reduce the appearance of building mass on a public street, and thereby reduce the visual impact of taller buildings.

VERTICAL RHYTHM or FAÇADE WIDTH - means the perceived width a building façade measured along a public street. Longer buildings may be designed to read as multiple buildings by the change of material, color, or other architectural elements.

Section 3

All ordinances or parts of ordinances inconsistent with this ordinance are hereby repealed to the extent of such inconsistencies.

Section 4

If any article, section, subsection, paragraph, phrase or sentence is, for any reason, held to be unconstitutional or invalid, said article, section, subsection, paragraph, phrase or sentence shall be deemed severable.

Section 5

This ordinance shall take effect immediately upon final publication as provided by law.

ATTEST:

TOWN OF DOVER, COUNTY OF MORRIS

Margaret J. Verga, Municipal Clerk

James P. Dodd, Mayor

Introduced: _____

Published: _____






Adopted: _____

The foregoing is a true copy of an ordinance adopted by the governing body of the Town of Dover, Morris County, New Jersey on _____, 2009.

Margaret J. Verga, Municipal Clerk

(SEAL)

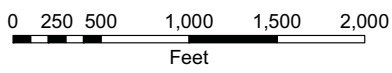
MAP LEGEND

-  MUNICIPAL BOUNDARY
-  ZONING DISTRICT BOUNDARY
-  BLACKWELL STREET HISTORIC DISTRICT AREA
-  PROPERTY LINES
-  WATERBODIES

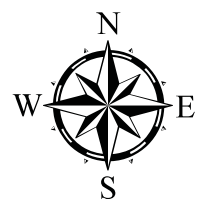
NOTES:
 1. THE "BLACKWELL STREET HISTORIC DISTRICT AREA" BOUNDARIES DEPICTED ON THIS MAP ARE FOR INFORMATIONAL PURPOSES ONLY FOR SHOWING ITS RELATIONSHIP TO THE SURROUNDING AND CONTAINING ZONING DISTRICTS. FOR ACTUAL REGULATORY DELINEATION, SEE THE "BLACKWELL STREET HISTORIC DISTRICT MAP" AS REFERENCED IN CHAPTER 236, ARTICLE VIIA OF THE CODE OF THE TOWN OF DOVER.
 2. THE "WATERBODIES" DEPICTED ON THIS MAP ARE FOR INFORMATIONAL PURPOSES ONLY AND ARE NOT REGULATORY IN NATURE.

ZONING DISTRICTS

- R-1 SINGLE FAMILY (7,500 S.F.)
- R-1S SINGLE FAMILY/STEEP SLOPE (30,000 S.F. /10,000 S.F.)
- R-2 SINGLE FAMILY (5,000 S.F.)
- R-3 DOUBLE FAMILY
- R-3A DOUBLE FAMILY / ROOMING HOUSE
- R-4 MULTI-FAMILY
- C-1 RETAIL COMMERCIAL
- C-2 GENERAL COMMERCIAL
- C-3 LIGHT INDUSTRIAL-COMMERCIAL
- D1 STATION AREA
- D2 BLACKWELL STREET HISTORIC
- D3 EAST BLACKWELL BUSINESS
- D4 SOUTH DOWNTOWN
- IND INDUSTRIAL
- IND/OP INDUSTRIAL-OFFICE PARK
- RAD REDEVELOPMENT AREA DISTRICT
- BHRPA BASSETT HIGHWAY REDEVELOPMENT PLAN AREA



Reduced Scale Print



ZONING MAP

TOWN OF DOVER

MORRIS COUNTY, NEW JERSEY

TOWN OF DOVER
 Engineering & Planning Department
 37 N. Sussex Street
 Dover, NJ 07801

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 Town Engineer & Planner

November, 2009