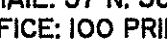
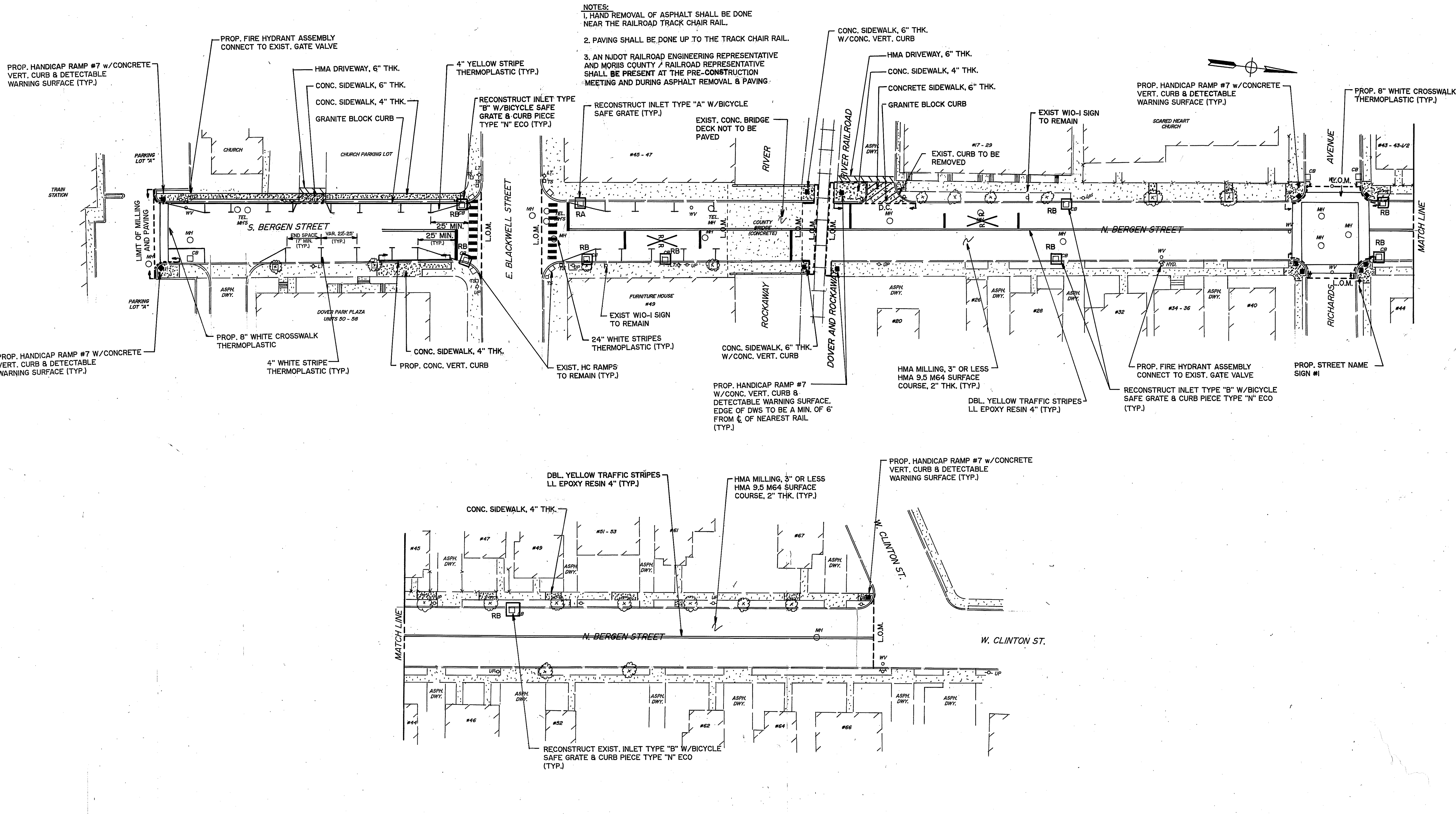
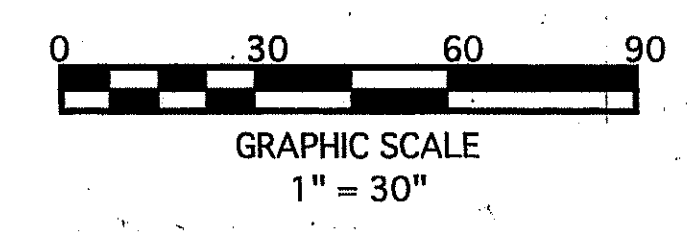


<b>TOWN OF DOVER</b> <b>ENGINEERING DEPARTMENT</b>  MAIL: 37 N. SUSSEX STREET OFFICE: 100 PRINCETON AVENUE DOVER, NEW JERSEY 07801  TEL. (973) 366-2200 ext. 2154/2152 FAX (973) 366-0039  Michael A. Hantson, PE, PP, CME - Town Engineer William J. Isselin - Assistant   <b>MICHAEL A. HANTSON - P.E.</b> N.J. PROFESSIONAL ENGINEER LICENSE NO. 27078	<b>COVER SHEET</b>  <b>CONSTRUCTION PLANS</b> <b>FOR</b> <b>N. &amp; S. BERGEN STREET</b> <b>ROADWAY IMPROVEMENTS</b> <b>TOWN OF DOVER</b> <b>MORRIS COUNTY, NEW JERSEY</b>						
DATE: <u>5/30/18</u>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">PROJECT NO. 2018-01</td> <td style="width: 50%;">SCALE: N.T.S.</td> </tr> <tr> <td colspan="2">SHEET <u>1</u> OF <u>6</u></td> </tr> <tr> <td colspan="2">REVISION: _____</td> </tr> </table>	PROJECT NO. 2018-01	SCALE: N.T.S.	SHEET <u>1</u> OF <u>6</u>		REVISION: _____	
PROJECT NO. 2018-01	SCALE: N.T.S.						
SHEET <u>1</u> OF <u>6</u>							
REVISION: _____							



- NOTES:
1. HAND REMOVAL OF ASPHALT SHALL BE DONE NEAR THE RAILROAD TRACK CHAIR RAIL.
  2. PAVING SHALL BE DONE UP TO THE TRACK CHAIR RAIL.
  3. AN NJDOT RAILROAD ENGINEERING REPRESENTATIVE AND MORRIS COUNTY / RAILROAD REPRESENTATIVE SHALL BE PRESENT AT THE PRE-CONSTRUCTION MEETING AND DURING ASPHALT REMOVAL & PAVING.

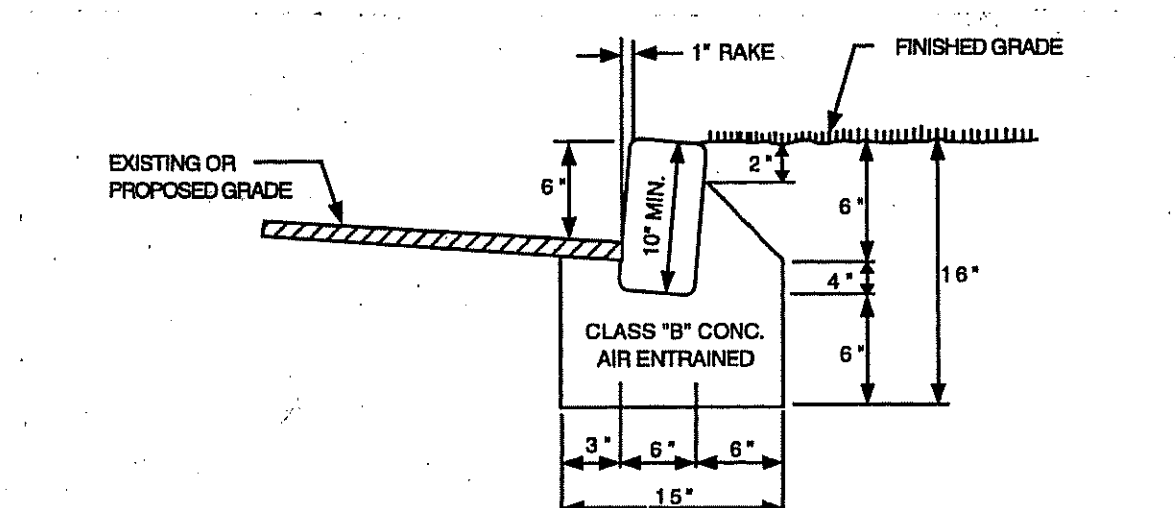
**N. & S. BERGEN STREET**



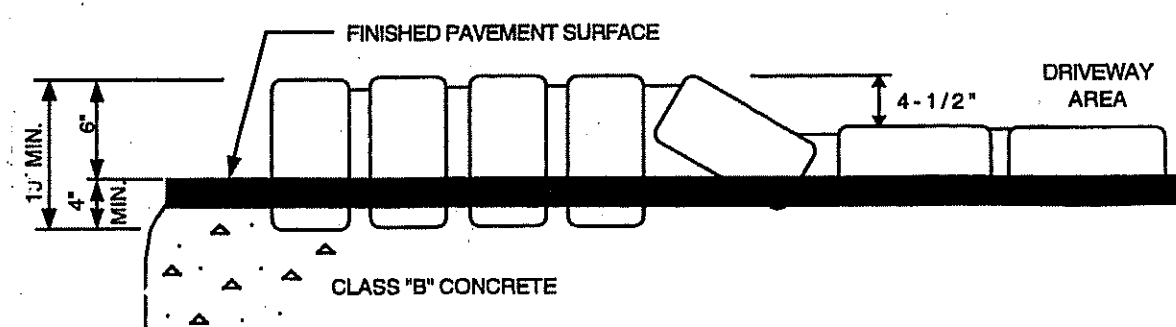
UNOFFICIAL COPY  
Not to Be Used For Bidding

<b>TOWN OF DOVER ENGINEERING DEPARTMENT</b> MAIL: 37 N. SUSSEX STREET OFFICE: 100 PRINCETON AVENUE DOVER, NEW JERSEY 07801 TEL: (973) 366-2200 ext. 2154/2152 FAX (973) 366-0039 Michael A. Hantson, PE, PP, CME - Town Engineer William J. Iselin - Assistant		<b>CONSTRUCTION PLAN</b> CONSTRUCTION PLANS FOR N. & S. BERGEN STREET ROADWAY IMPROVEMENTS TOWN OF DOVER MORRIS COUNTY, NEW JERSEY	
PROJECT NO. 2018-01		SCALE: 1" = 30'	
SHEET 2 OF 6		REV. NO.:	
DATE: 8/17/18		REVISION:	

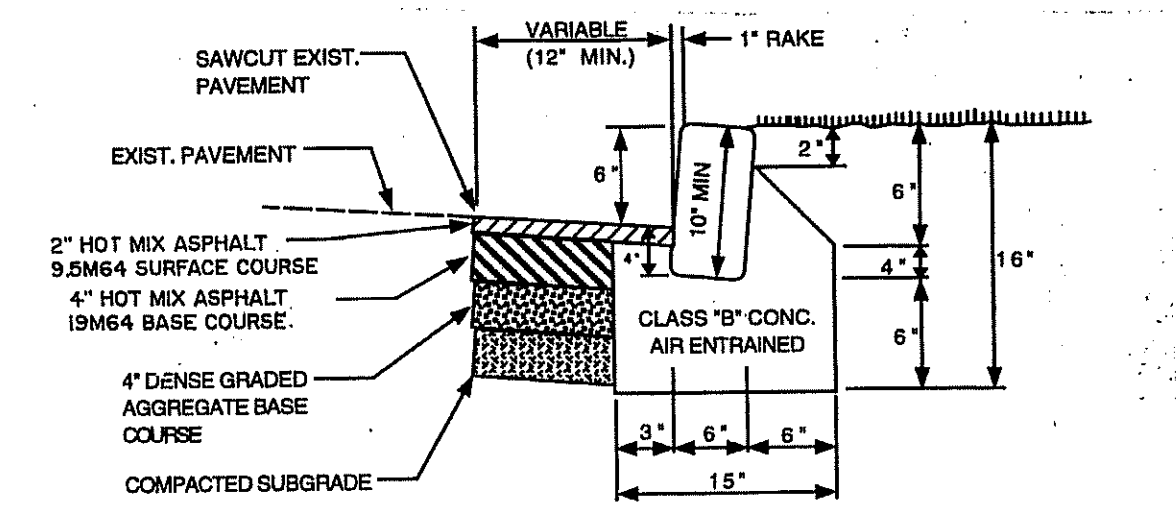




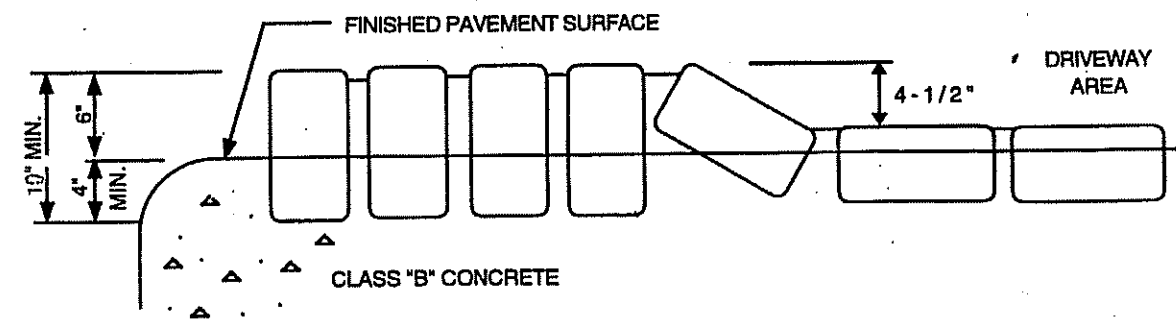
**NOTES:**  
 1. DENSE GRADED AGGREGATE BASE COURSE SHALL BE USED IF REQUIRED TO MAKE ANY GRADE ADJUSTMENTS.  
 2. JOINTS ARE TO BE 3/4" WIDE AND POINTED WITH 1:2 MIX CEMENT MORTAR. JOINTS TO BE CLEANED PRIOR TO POINTING.



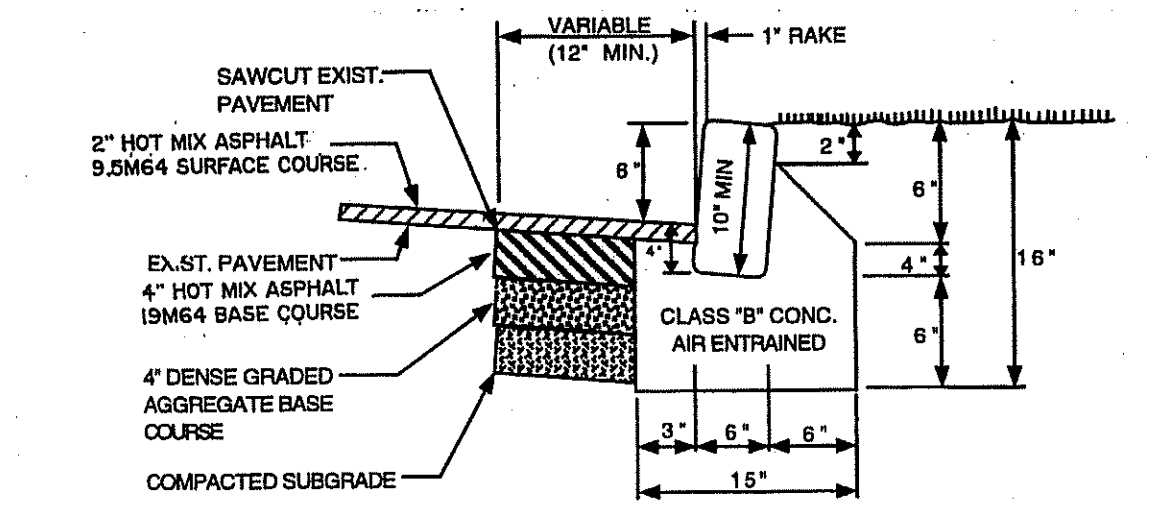
**GRANITE BLOCK CURB**



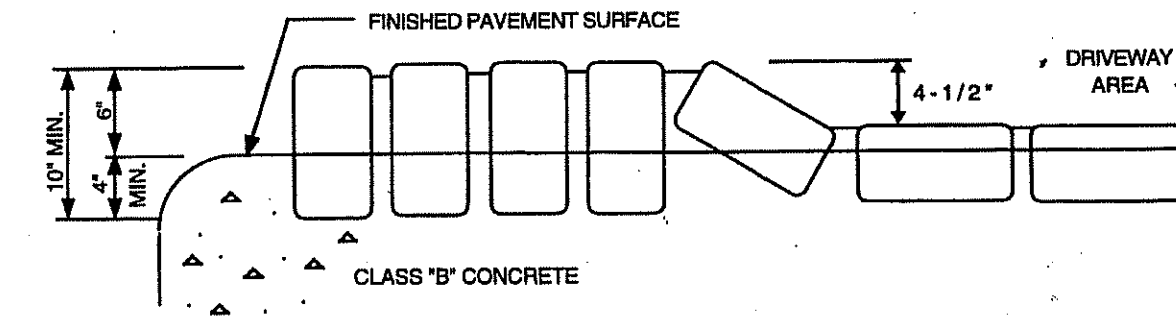
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 2. JOINTS ARE TO BE 3/4" WIDE AND POINTED WITH 1:2 MIX CEMENT MORTAR. JOINTS TO BE CLEANED PRIOR TO POINTING.



**GRANITE BLOCK CURB / PAVEMENT RESTORATION**

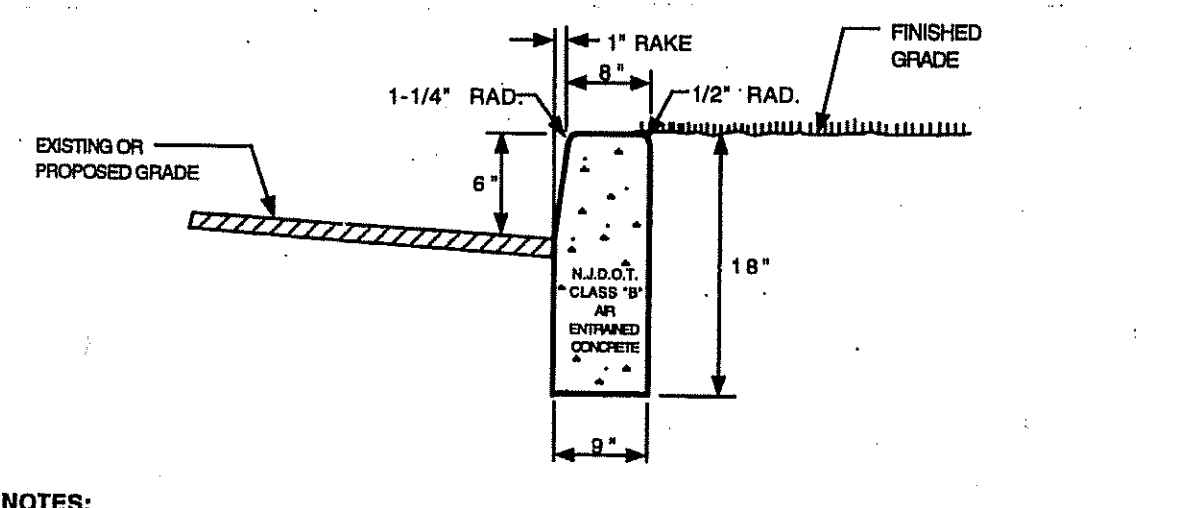


**NOTES:**  
 1. DENSE GRADED AGGREGATE BASE COURSE SHALL BE USED IF REQUIRED TO MAKE ANY GRADE ADJUSTMENTS.  
 2. JOINTS ARE TO BE 3/4" WIDE AND POINTED WITH 1:2 MIX CEMENT MORTAR. JOINTS TO BE CLEANED PRIOR TO POINTING.

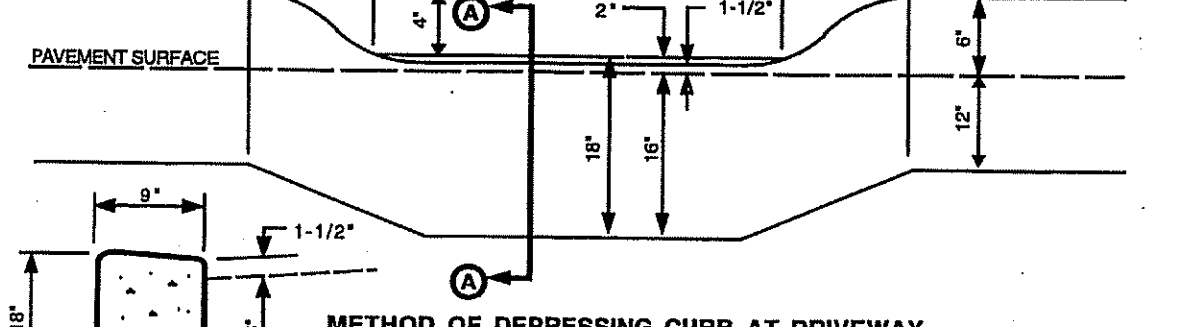


**GRANITE BLOCK CURB / PAVEMENT RESTORATION**

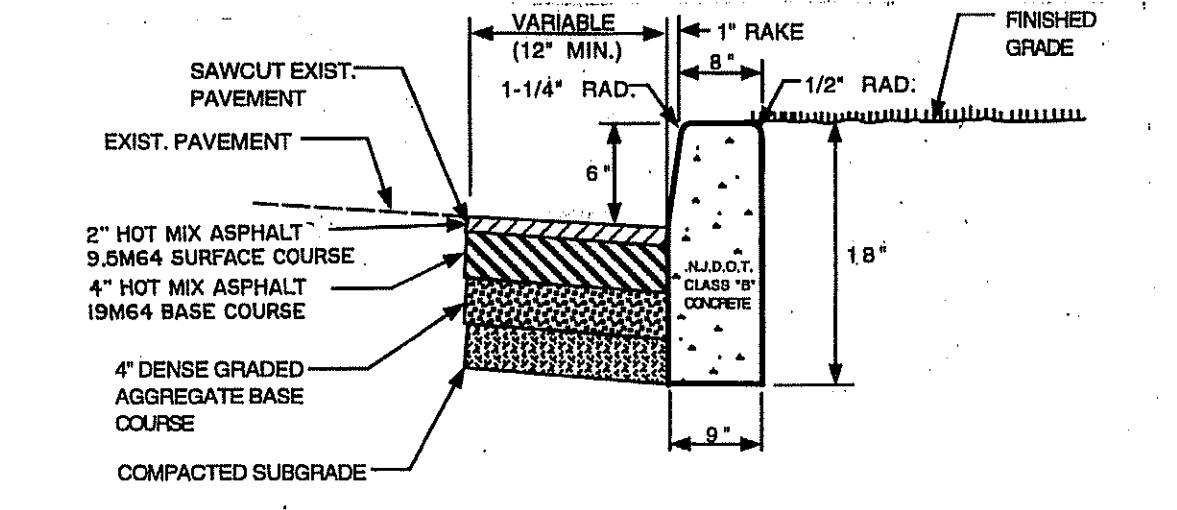
**CURB NOTE:**  
 THE COST FOR PAVEMENT RESTORATION FOR INSTALLATION OF CURBS SHALL BE INCLUDED IN THE LINEAR FOOT PRICE FOR CURB.



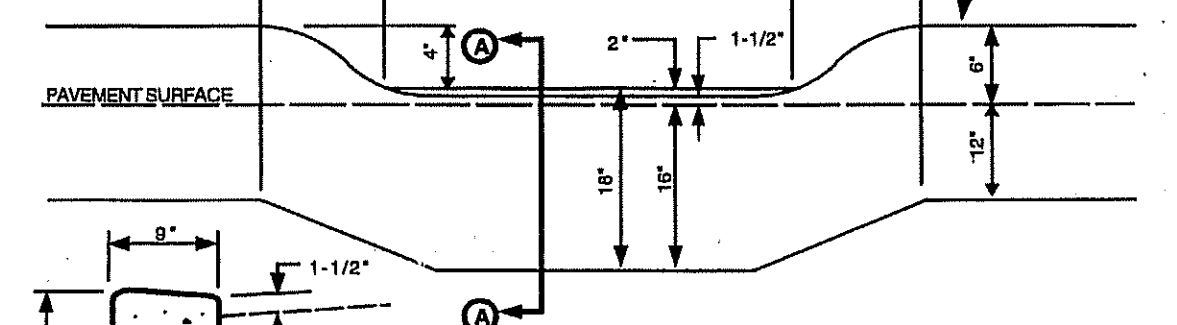
**NOTES:**  
 1. TRANSVERSE JOINTS 1/2" WIDE SHALL BE INSTALLED IN THE CURB 20' APART AND SHALL BE FILLED WITH PREFORMED BITUMINOUS IMPREGNATED FIBER JOINT FILLER, COMPLYING WITH THE REQUIREMENTS OF A.A.S.H.O. SPEC. M-213, RECESSED 1/4" IN FROM THE FRONT FACE AND TOP OF CURB.



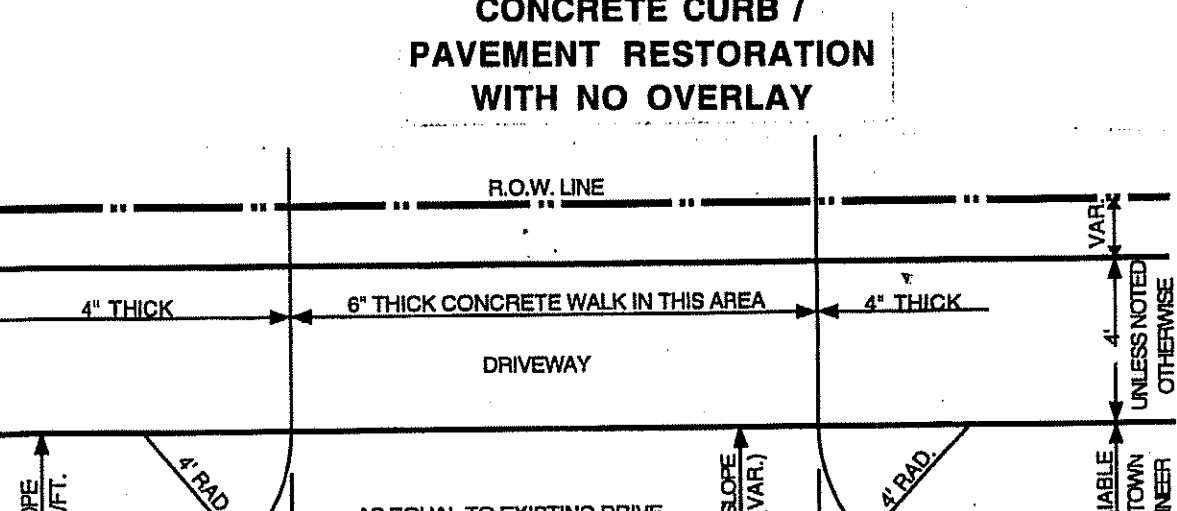
**CONCRETE CURB**



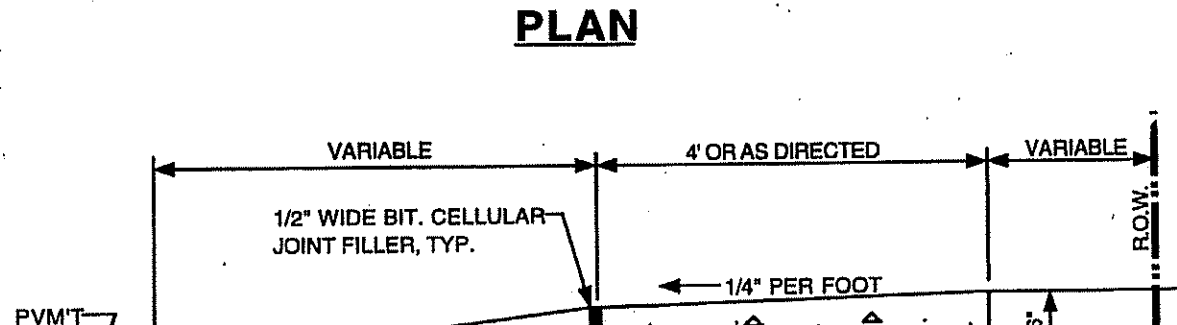
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**CONCRETE CURB / PAVEMENT RESTORATION**

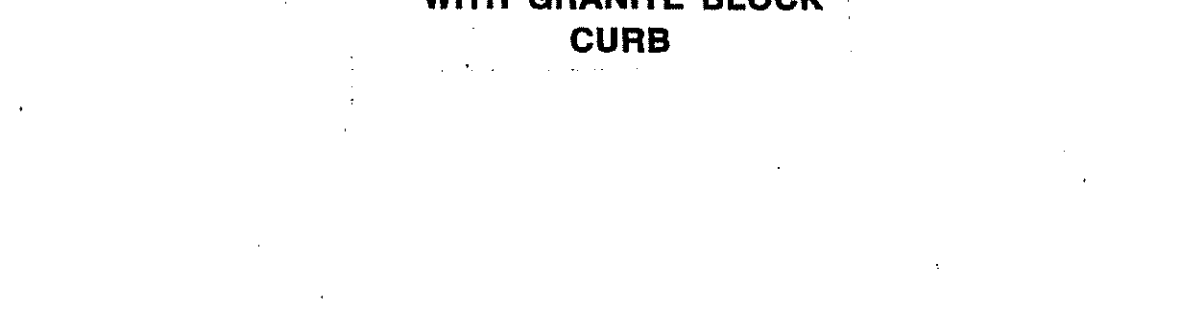


**NOTES:**  
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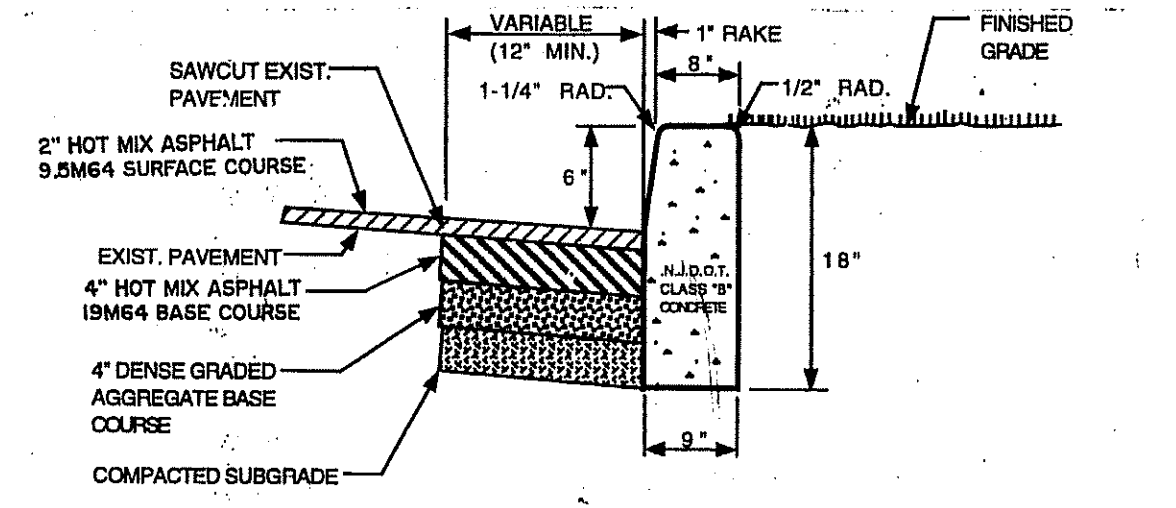


**CONCRETE CURB / PAVEMENT RESTORATION WITH NO OVERLAY**

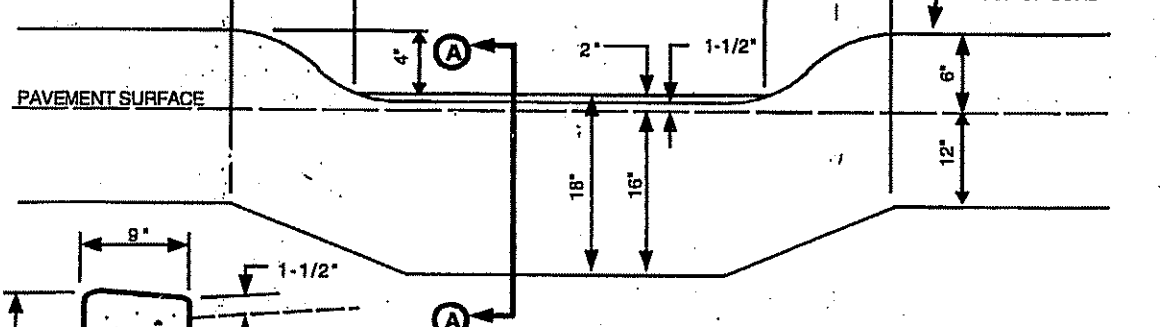
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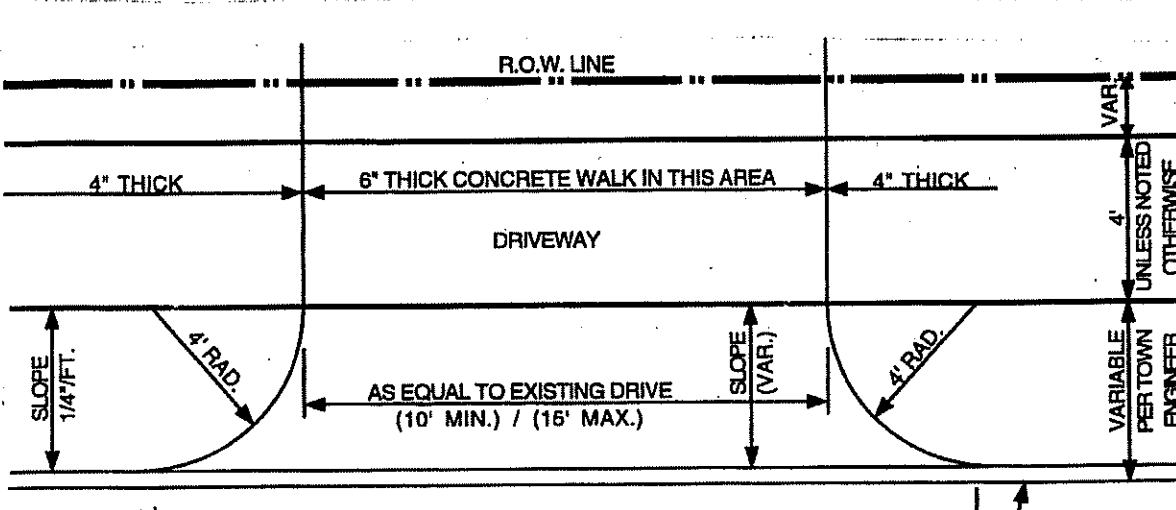
**CONCRETE DRIVEWAY WITH GRANITE BLOCK CURB**



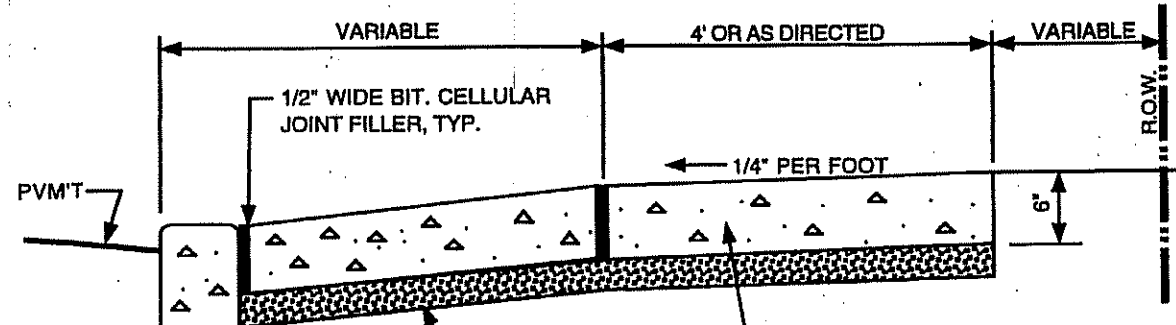
**NOTES:**  
 1. TRANSVERSE JOINTS 1/2" WIDE SHALL BE INSTALLED IN THE CURB 20' APART AND SHALL BE FILLED WITH PREFORMED BITUMINOUS IMPREGNATED FIBER JOINT FILLER, COMPLYING WITH THE REQUIREMENTS OF A.A.S.H.O. SPEC. M-213, RECESSED 1/4" IN FROM THE FRONT FACE AND TOP OF CURB.



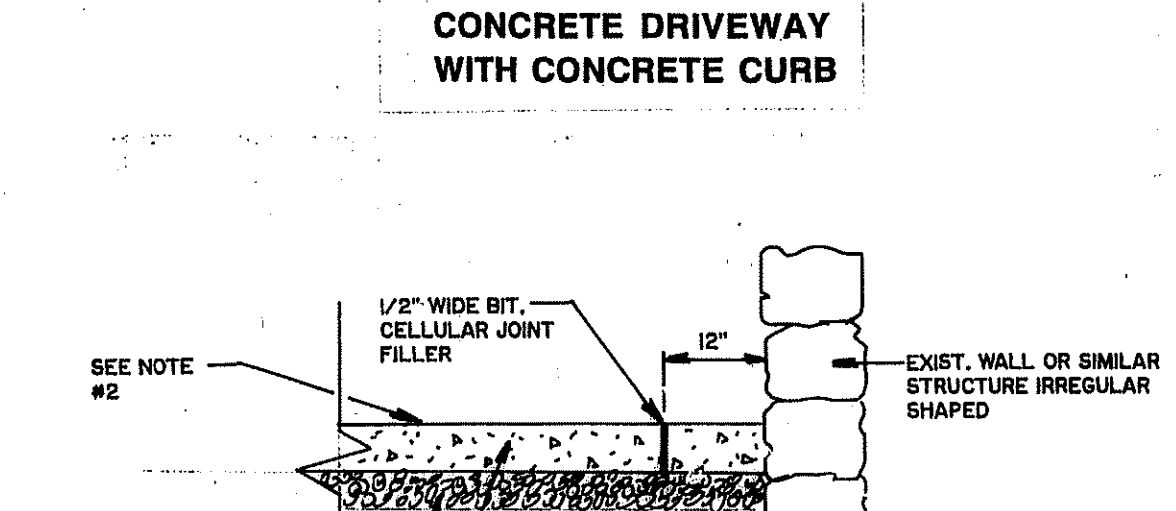
**CONCRETE CURB / PAVEMENT RESTORATION**



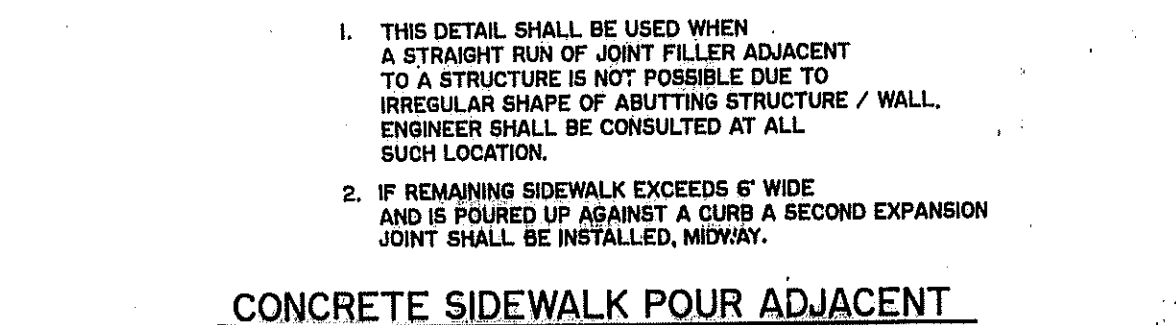
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**CONCRETE CURB / PAVEMENT RESTORATION**

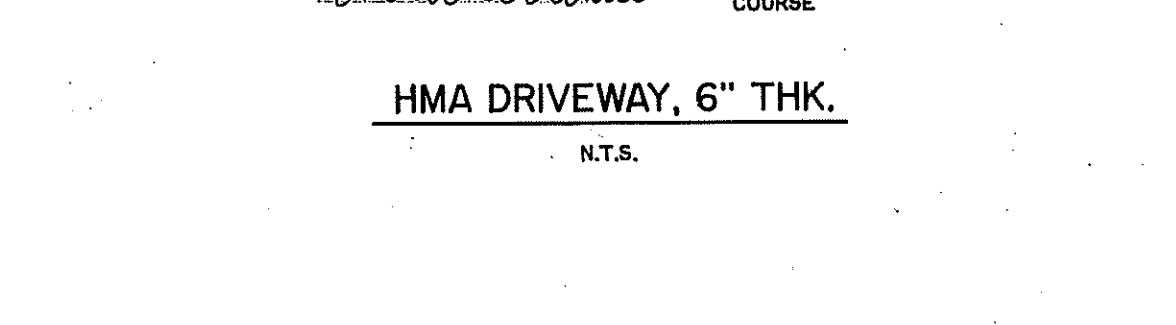


**NOTES:**  
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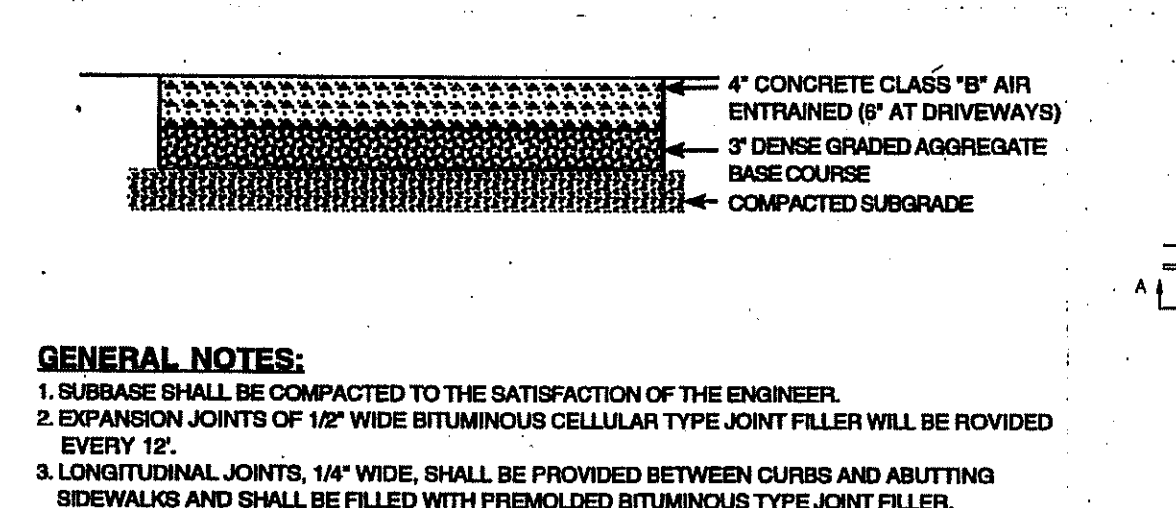


**CONCRETE DRIVEWAY WITH CONCRETE CURB**

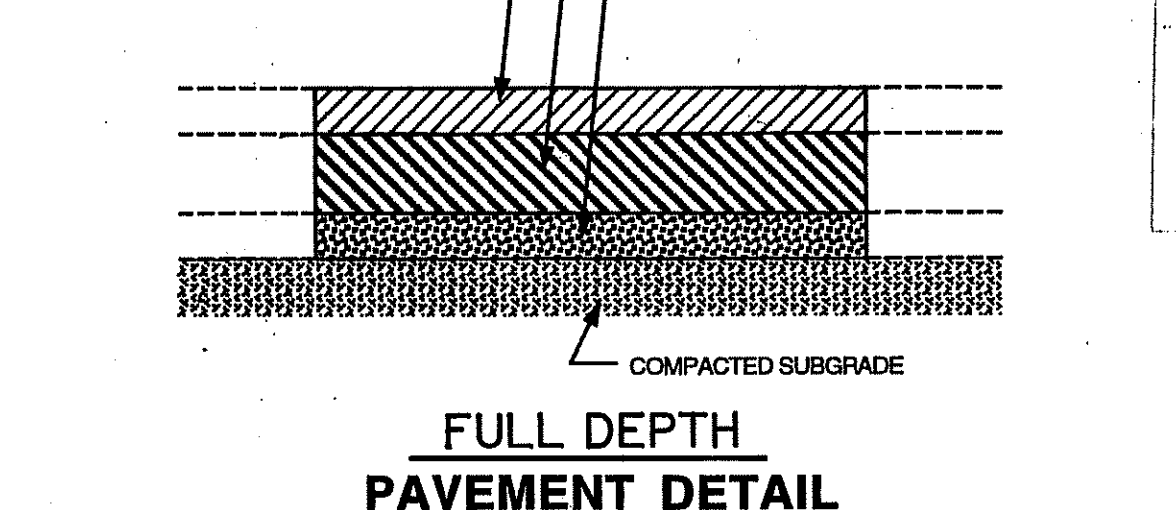
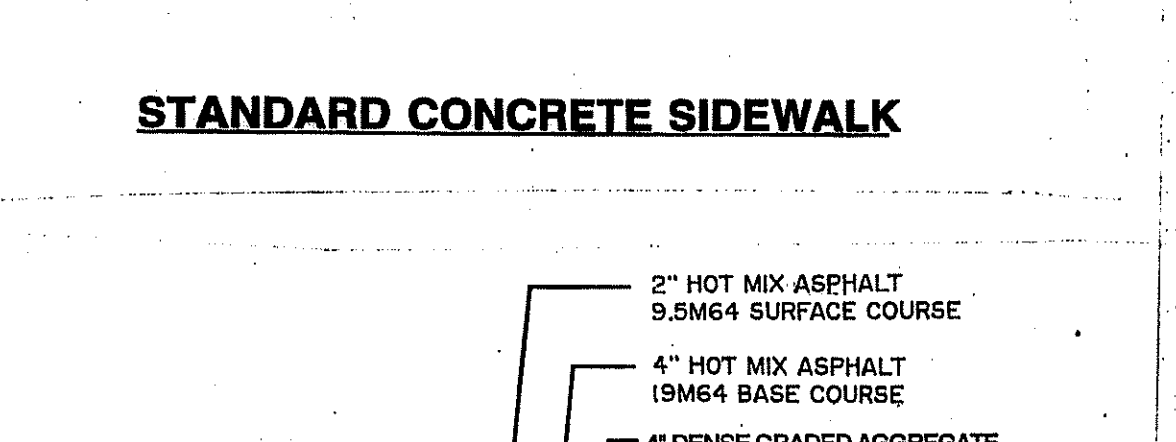
**NOTES:**  
 1. TRANSVERSE JOINTS 1/2" WIDE SHALL BE INSTALLED IN THE CURB 20' APART AND SHALL BE FILLED WITH PREFORMED BITUMINOUS IMPREGNATED FIBER JOINT FILLER, COMPLYING WITH THE REQUIREMENTS OF A.A.S.H.O. SPEC. M-213, RECESSED 1/4" IN FROM THE FRONT FACE AND TOP OF CURB.



**HMA DRIVEWAY, 6" THK.**

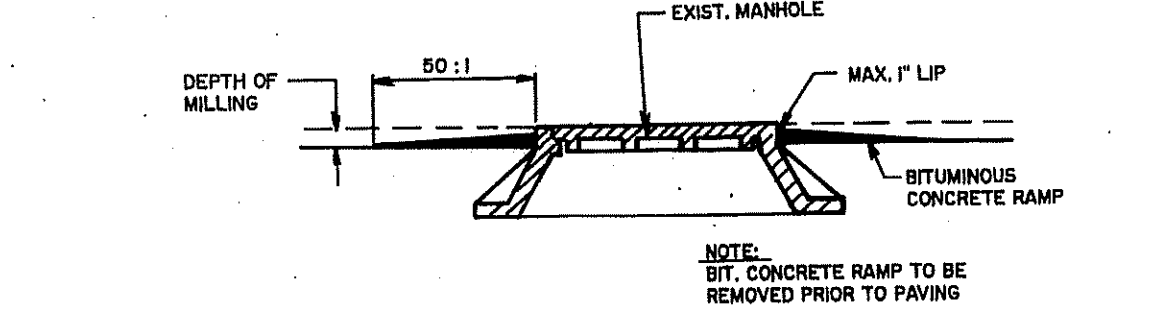


**GENERAL NOTES:**  
 1. SUBBASE SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER.  
 2. EXPANSION JOINTS OF 1/2" WIDE BITUMINOUS CELLULAR TYPE JOINT FILLER WILL BE PROVIDED EVERY 12'.  
 3. LONGITUDINAL JOINTS, 1/4" WIDE, SHALL BE PROVIDED BETWEEN CURBS AND ABUTTING SIDEWALKS AND SHALL BE FILLED WITH PREMOULDED BITUMINOUS TYPE JOINT FILLER.  
 4. TRANSVERSE SURFACE GROOVES SHALL BE CUT IN SIDEWALKS BETWEEN EXPANSION JOINTS AT INTERVALS EQUAL TO THE SIDEWALK WIDTH.  
 5. ALL CONCRETE SHALL BE CLASS 'B' AIR ENTRAINED.  
 6. ALL JOINTS AND EDGES ARE TO BE TOoled.  
 7. SURFACE SHALL BE BRUSHED TRANSVERSELY TO A NEAT FINISH.  
 8. STANDARD CONCRETE SIDEWALK MEETS N.J.D.O.T. STANDARDS.

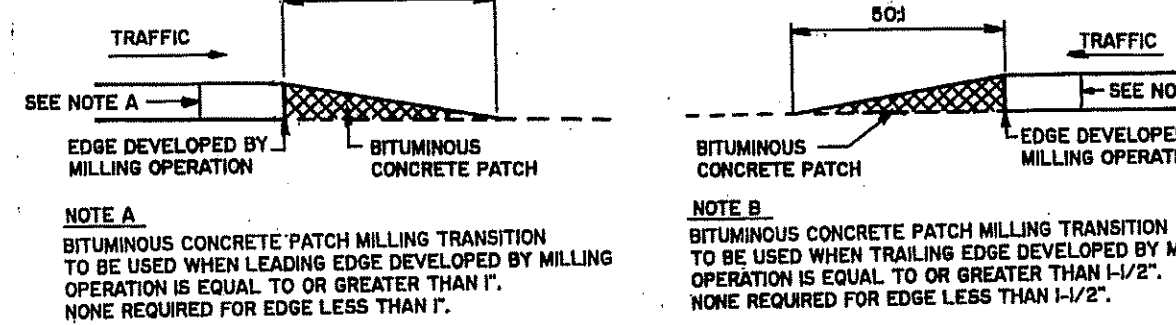
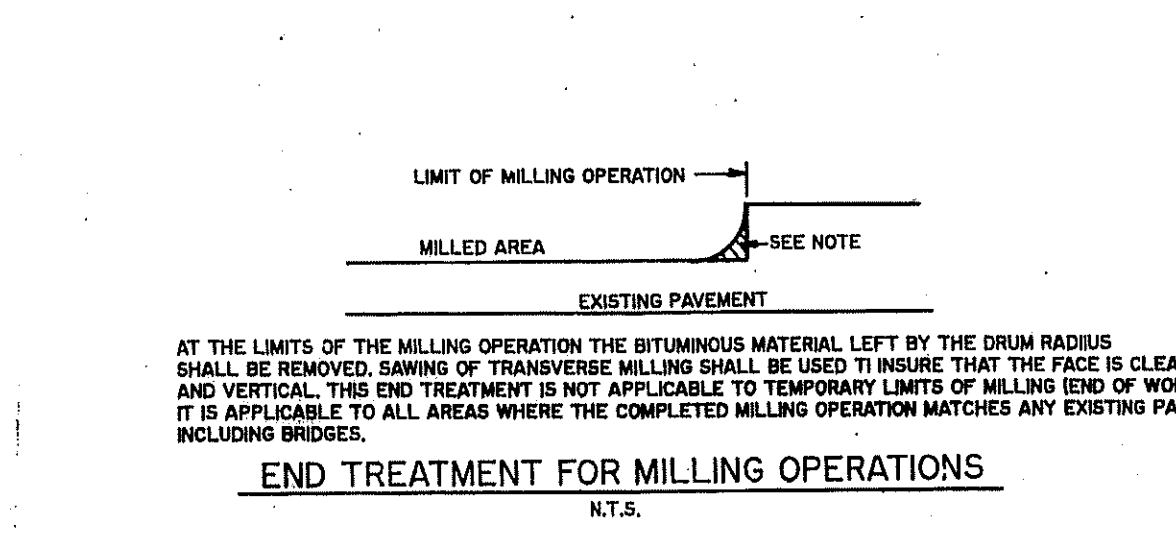


**FULL DEPTH PAVEMENT DETAIL**

**NOTE:**  
 1. ALL MATERIALS SHALL CONFORM TO THE NEW JERSEY D.O.T. STANDARD SPECIFICATIONS FOR ROAD & BRIDGE CONSTRUCTION, 2007 EDITION.



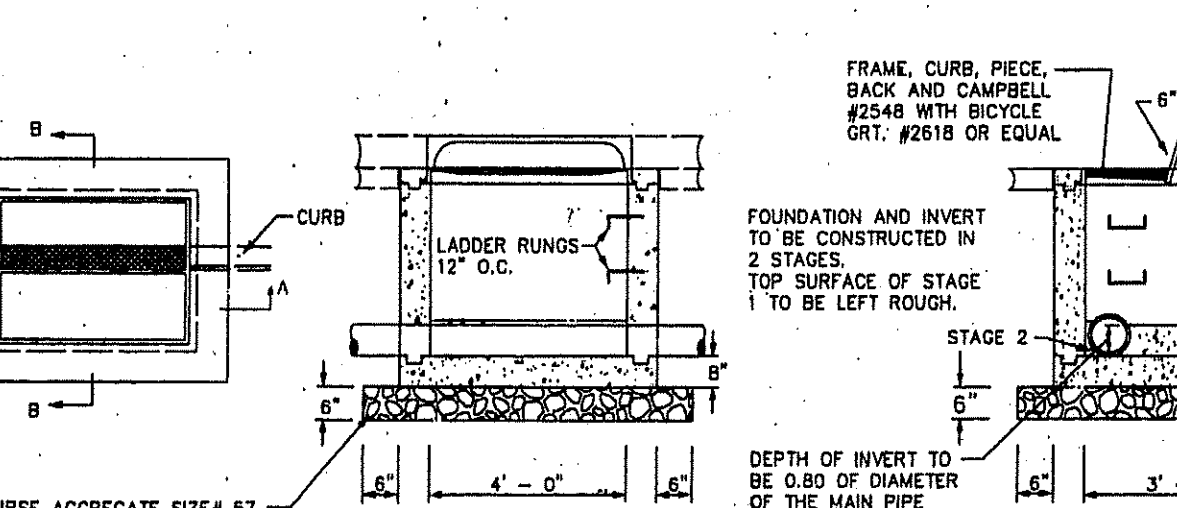
**MANHOLE BITUMINOUS CONCRETE RAMP DETAIL**



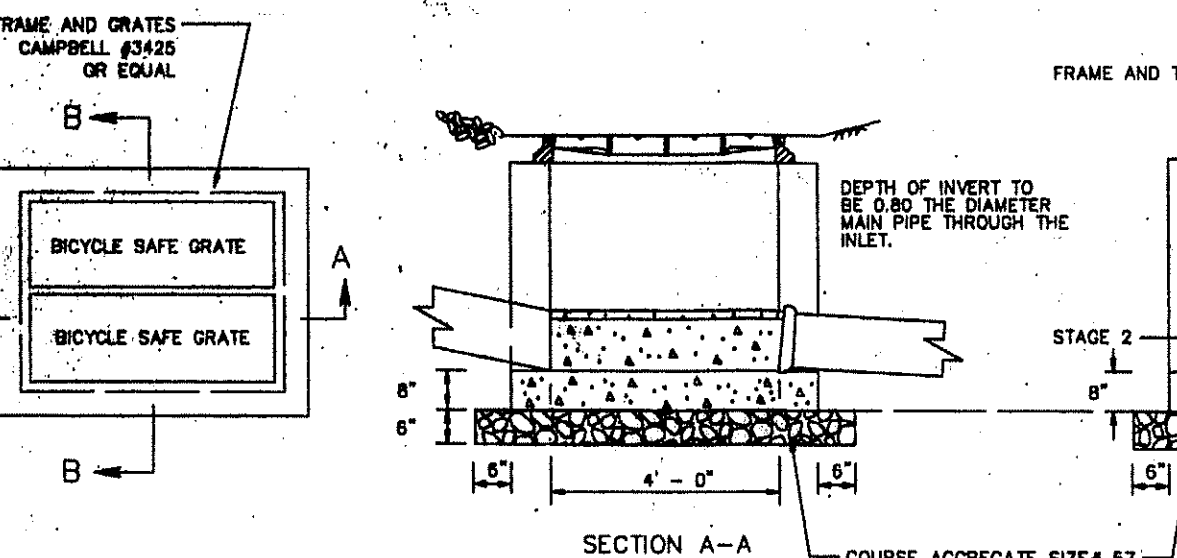
**MILLING TRANSITIONS**



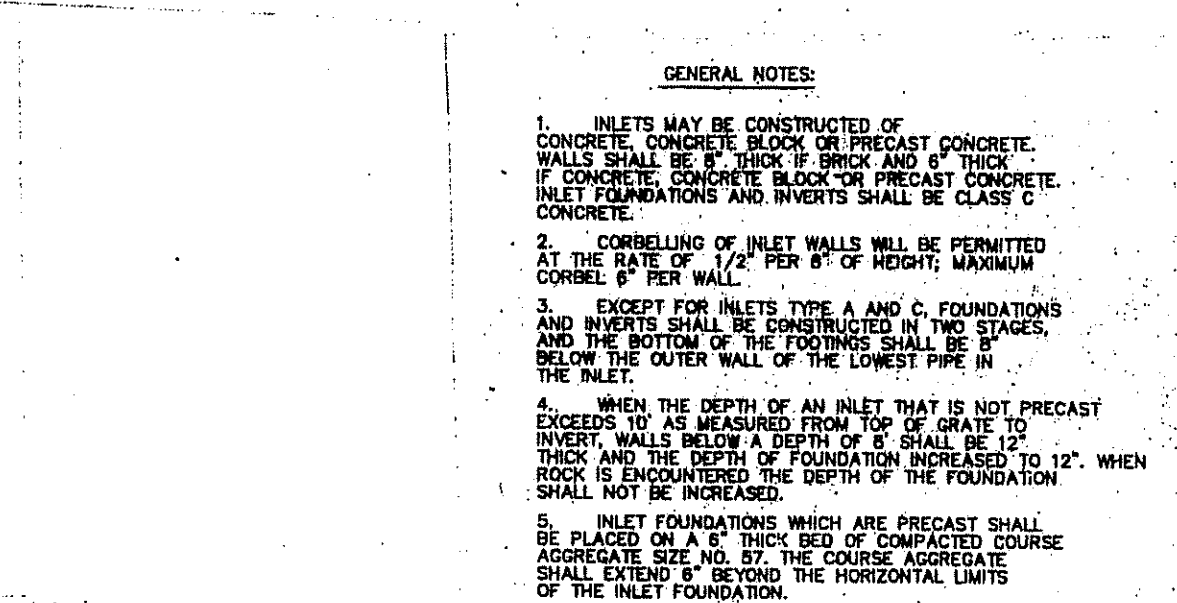
**MILLING AND PAVING SECTION**



**GENERAL NOTES:**  
 1. INLETS MAY BE CONSTRUCTED OF CONCRETE, CONCRETE BLOCK OR PRECAST CONCRETE. WALLS SHALL BE 8" THICK IF BRICK AND 6" THICK IF CONCRETE. CONCRETE BLOCK OR PRECAST CONCRETE INLET FOUNDATIONS AND INVERTS SHALL BE CLASS C CONCRETE.  
 2. CORRELING OF INLET WALLS WILL BE PERMITTED AT THE RATE OF 1/2" PER 8" OF HEIGHT; MAXIMUM CORREL 6" PER WALL.  
 3. EXCEPT FOR INLETS TYPE A AND C, FOUNDATIONS AND INVERTS SHALL BE CONSTRUCTED IN 2 STAGES. THE DEPTH OF THE FOUNDATION SHALL BE 8" BELOW THE OUTER WALL OF THE LOWEST PIPE IN THE INLET.  
 4. WHEN THE DEPTH OF AN INLET THAT IS NOT PRECAST EXCEEDS 10' AS MEASURED FROM TOP OF GRATE TO INVERT WALLS BELOW A 6" THICK BED OF COMPACTED COURSE AGGREGATE SHALL BE INCREASED TO 12" WHEN ROCK IS ENCOUNTERED THE DEPTH OF THE FOUNDATION SHALL NOT BE INCREASED.  
 5. INLET FOUNDATIONS WHICH ARE PRECAST SHALL BE PLACED ON A 6" THICK BED OF COMPACTED COURSE AGGREGATE SIZE NO. 57. THE COURSE AGGREGATE SHALL EXTEND 6" BEYOND THE HORIZONTAL LIMITS OF THE INLET FOUNDATION.  
 6. EXISTINGS FOR PRECAST INLETS SHALL BE ADJUSTED TO 12" MAXIMUM.  
 7. WHEN THE DEPTH OF A PRECAST INLET EXCEEDS 10' AS MEASURED FROM TOP OF GRATE TO INVERT, THE FOUNDATION SHALL BE INCREASED TO 12" WHEN ROCK IS ENCOUNTERED THE DEPTH OF THE FOUNDATION SHALL NOT BE INCREASED.  
 8. MINIMUM WALL REINFORCEMENT FOR PRECAST INLETS TYPES A, B, C, E, D-1, D-2 AND B MODIFIED:  
 DEPTH BELOW REIN. WALL THK.  
 >10'-1" TO <15'-0" #4 @ 10" C.C. #4 @ 18" C.C. 6" THK.  
 >15'-1" TO <20'-0" #4 @ 8" C.C. #4 @ 18" C.C. 6" THK.  
 REINFORCING SHOWN FOR PRECAST INLETS IS THE MINIMUM REQUIRED. ADDITIONAL REINFORCING FOR HANDLING IS THE RESPONSIBILITY OF THE CONTRACTOR.  
 ALTERNATE REINFORCEMENT  
 9. COST FOR PAVEMENT REPAIR FOR PIPE AND INLET CONSTRUCTION TO BE INCLUDED IN THE COST FOR THE VARIOUS DRAINAGE ITEMS.  
 NOTE (ALL INLETS):  
 CONTRACTOR TO VERIFY HEIGHT OF CURB FACE BEFORE ORDERING CASTING.



**INLETS, TYPE E**



**STORM SEWER TRENCH DETAIL**

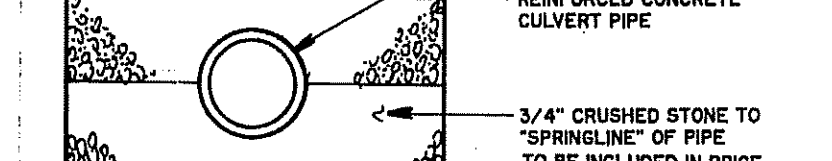
DEPTH BELOW REIN. WALL THK.	HORIZONTAL REIN.	VERTICAL REIN.	WALL THK.
>10'-1" TO <15'-0"	#4 @ 10" C.C.	#4 @ 18" C.C.	6" THK.
>15'-1" TO <20'-0"	#4 @ 8" C.C.	#4 @ 18" C.C.	6" THK.

**REINFORCING SHOWN FOR PRECAST INLETS IS THE MINIMUM REQUIRED. ADDITIONAL REINFORCING FOR HANDLING IS THE RESPONSIBILITY OF THE CONTRACTOR.**  
**ALTERNATE REINFORCEMENT**  
 9. COST FOR PAVEMENT REPAIR FOR PIPE AND INLET CONSTRUCTION TO BE INCLUDED IN THE COST FOR THE VARIOUS DRAINAGE ITEMS.  
**NOTE (ALL INLETS):**  
 CONTRACTOR TO VERIFY HEIGHT OF CURB FACE BEFORE ORDERING CASTING.



**STORM SEWER TRENCH DETAIL**

**NOTE (ALL INLETS):**  
 CONTRACTOR TO VERIFY HEIGHT OF CURB FACE BEFORE ORDERING CASTING.



**STORM SEWER TRENCH DETAIL**

**NOTE (ALL INLETS):**  
 CONTRACTOR TO VERIFY HEIGHT OF CURB FACE BEFORE ORDERING CASTING.

**STORM SEWER TRENCH DETAIL**

TOWN OF DOVER ENGINEERING DEPARTMENT		CONSTRUCTION DETAILS	
MAIL: 57 N. SUSSEX STREET OFFICE: 100 PRINCETON AVENUE DOVER, NEW JERSEY 07801		CONSTRUCTION PLANS FOR N. & S. BERGEN STREET ROADWAY IMPROVEMENTS TOWN OF DOVER MORRIS COUNTY, NEW JERSEY	
TEL. (973) 366-2200 ext. 2154/2152 FAX (973) 366-0039 Michael A. Hantson, PE, PP, CME - Town Engineer William J. Ieselin - Assistant		PROJECT NO. 2018-01 SCALE: AS SHOWN	
MICHAEL A. HANTSON - P.E. N.J. PROFESSIONAL ENGINEER LICENSE NO. 27678		SHEET 3 OF 6 REVISION:	

DATE:	REVISION:

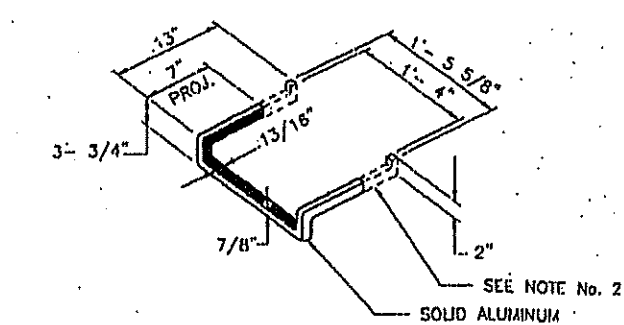
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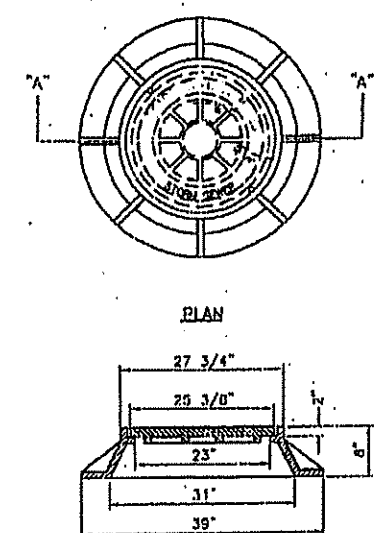
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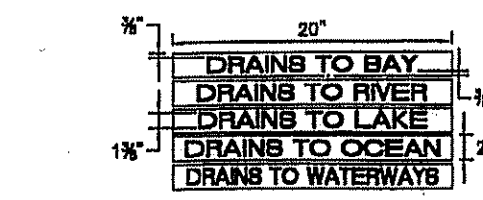
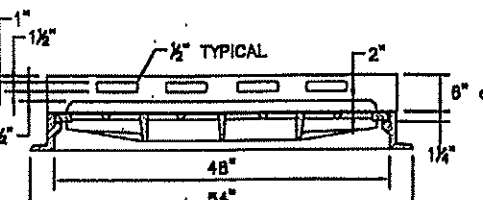
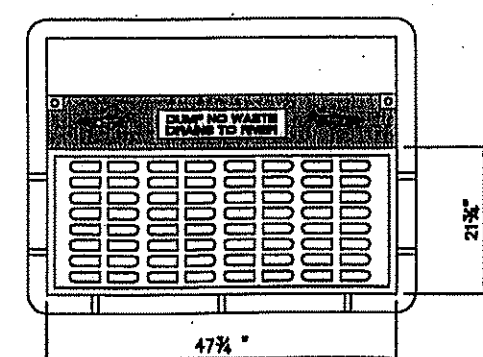


- NOTES:
1. ALUMINUM STEPS SHALL BE EXTRUDED ALUMINUM 6061-T6 ALLOY DROP FRONT DESIGN OR APPROVED EQUAL.
  2. THE PORTION TO BE IMBEDDED IN THE CONCRETE SHALL BE COATED WITH COAL TAR PITCH OR OTHER APPROVED MATERIAL AND SHALL BE IN ACCORDANCE WITH THE LATEST O.S.J.A. STANDARDS (OF MINIMUM IMBEDMENT).
  3. ALL MANHOLE STEPS TO MEET OR EXCEED ASTM AND O.S.J.A. REQUIREMENTS.

ALUMINUM STEP DETAIL

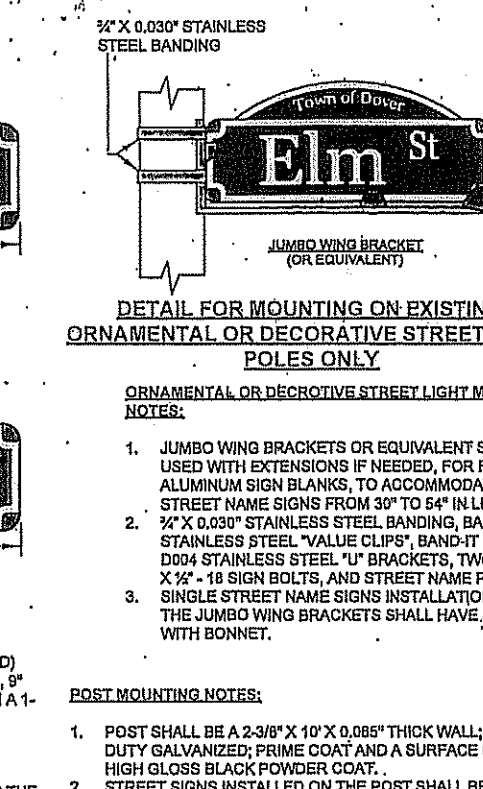
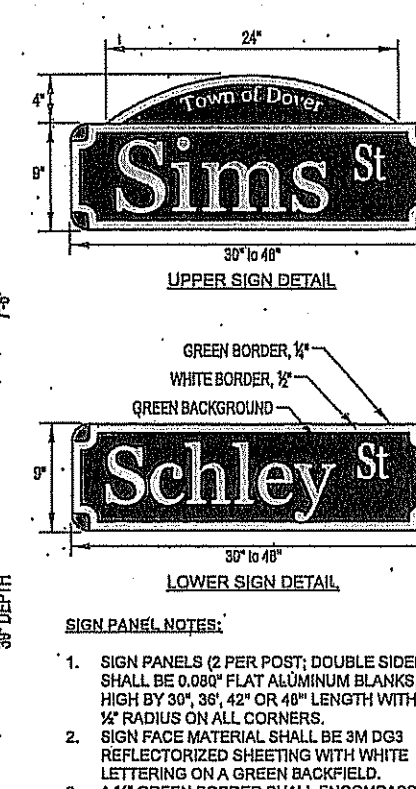
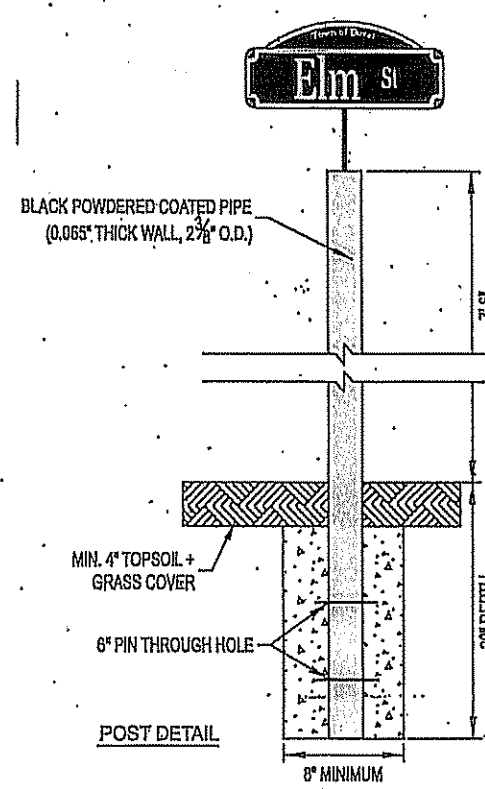


STANDARD MANHOLE FRAME AND COVER



TYPE 'B' INLET WITH BICYCLE SAFE GRATE AND ECO CURB PIECE (TYPE 'N')

(CAMPELL FOUNDRY COMPANY PATTERN No. 2818) OR APPROVED EQUAL  
N.T.S.

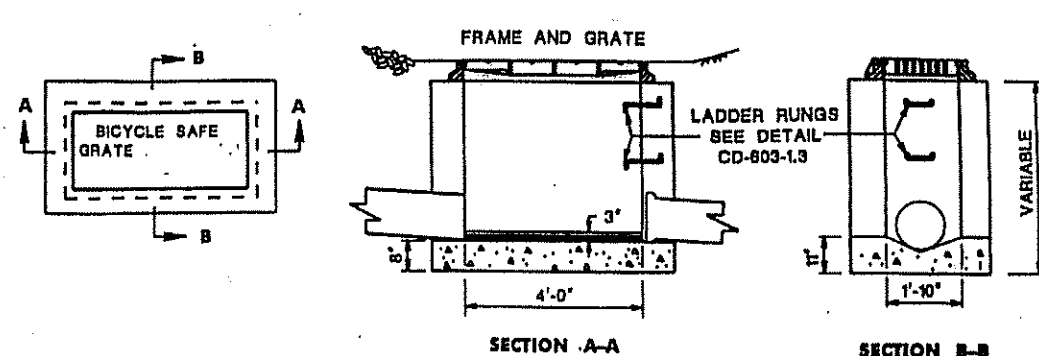


STREET NAME SIGNS

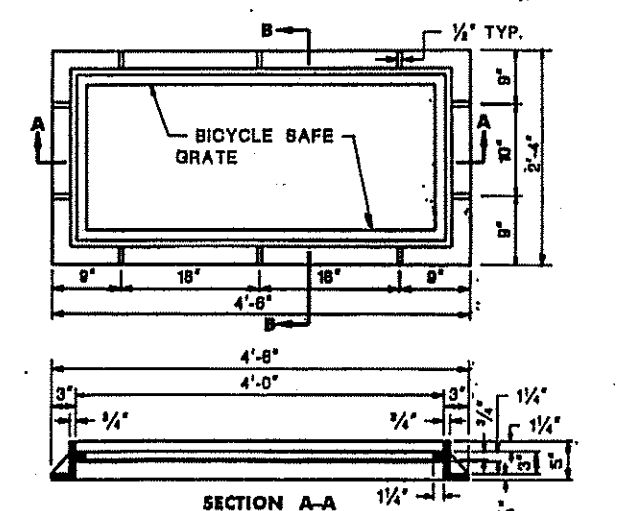
STREET SIGNS				
SIGN NO.	TOP SIGN W/BONNET	BOTTOM SIGN		COMMENTS
1	STREET NAME	SIGN WIDTH	STREET	SIGN WIDTH
1	N. BERGEN ST.	30"	RICHARDS AVE.	30"

LAYOUT EXAMPLE (BORDERS NOT SHOWN):

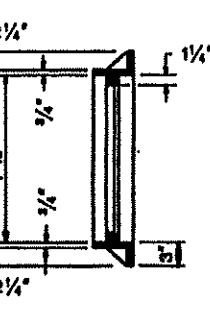
N. Bergen St



INLETS, TYPE A

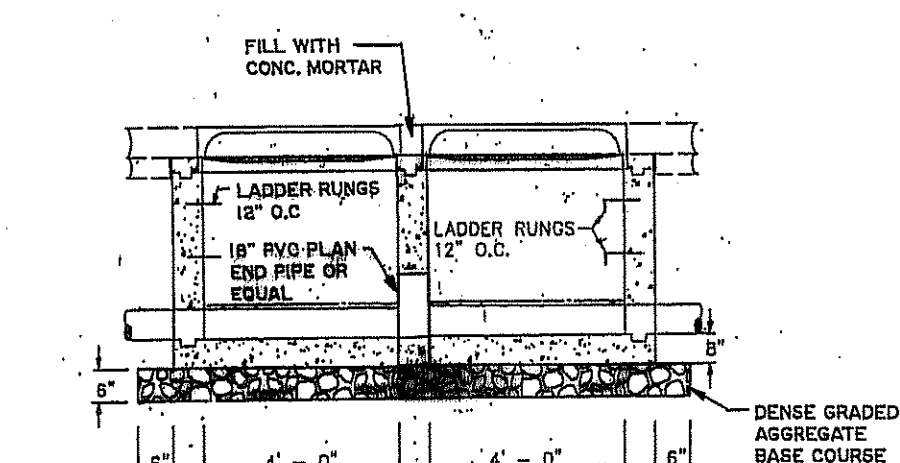


FRAME FOR INLETS, TYPE A

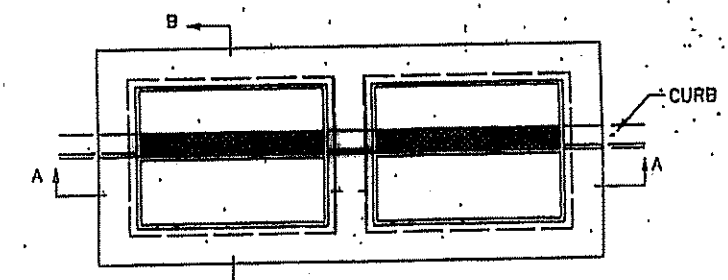


SECTION B-B

MAXIMUM WEIGHTS  
WEIGHT OF FRAME = 240#  
WEIGHT OF GRATE = 325#



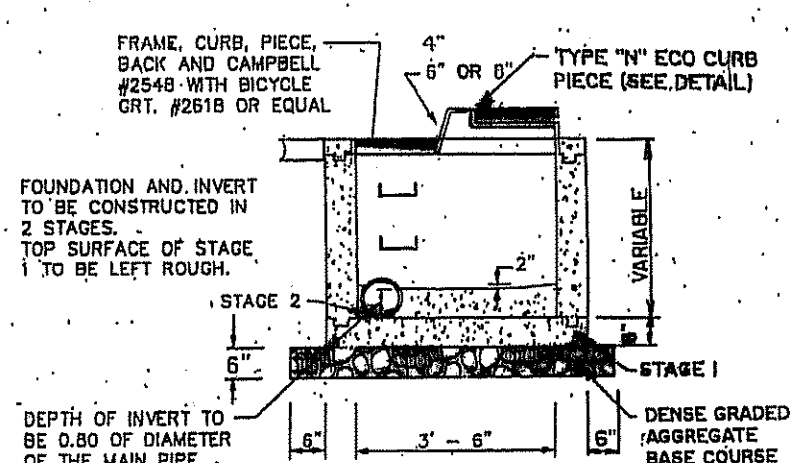
SECTION "A - A"



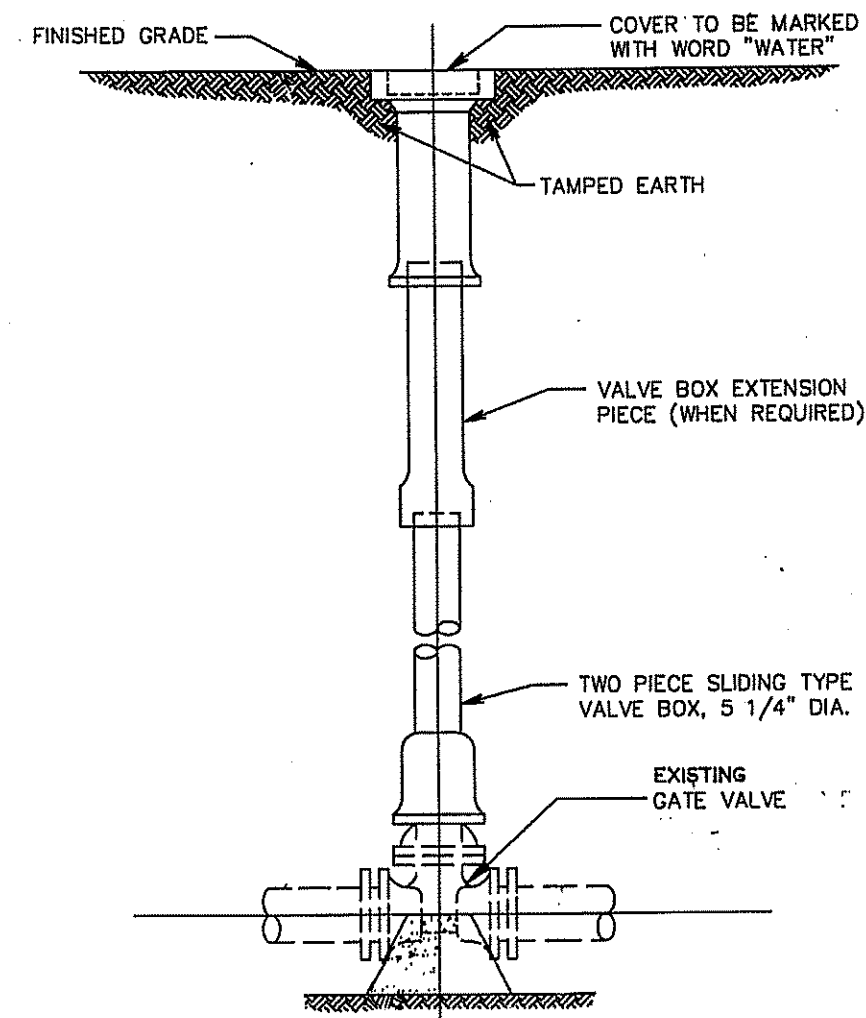
TOP VIEW

INLET, TYPE DBL. "B"

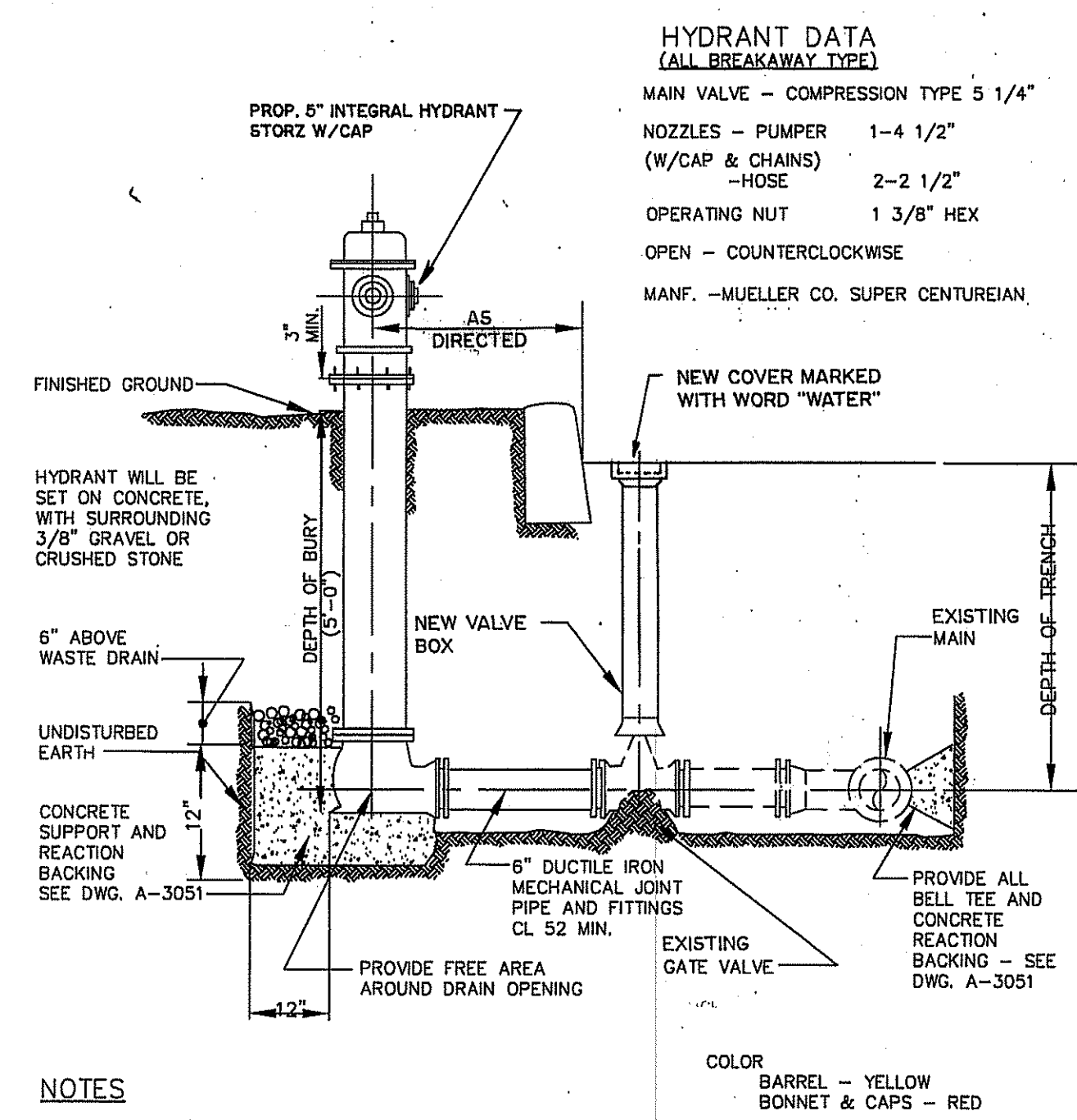
N.T.S.



SECTION "B - B"



N.T.S.



N.T.S.

NOTES

1. FOR MATERIAL SPECIFICATIONS REFER TO SPECIFICATIONS
2. Hydrants shall conform to the requirements of the American Water Works Association "Dry-Barrel Fire Hydrants" AWWA C502 latest revision. The minimum valve opening shall be five and one quarter (5-1/4") inches. The hydrant shall have a two section barrel with breakable flange and stem coupling. The hydrant valve shall open counterclockwise. Installation details for hydrants are shown on Drawing No. A-3047.

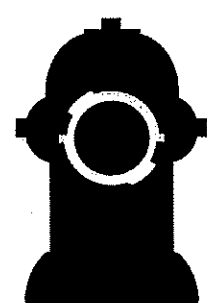
HIHS™ SPECIFICATION

The Harrington Integral Hydrant Storz nozzle is provided on new fire hydrants. The HIHS™ meets or exceeds the requirements of AWWA C502 regarding material and pressure testing. A Storz spanner wrench is required for cap removal.

The Storz shall have a brass metal face and hard anodized aluminum Storz ramps and lugs. The aluminum finish shall be hardcoat anodized to MIL-A-8625, Type 3, dark gray. The Storz shall be made of forged 6061-T6 aluminum.

The blind cap shall have hard anodized aluminum Storz ramps and lugs, made of forged 6061-T6 aluminum. The forged cap shall be equipped with a Storz seal. The cap shall be connected to the hydrant with a 0.125" vinyl coated aircraft cable. The high torque cap requires a Storz spanner wrench for removal. Once installed, the Harrington Integral Hydrant Storz with cap extends less than 2" from the hydrant nozzle.

Specify: HARRINGTON "HIHS"



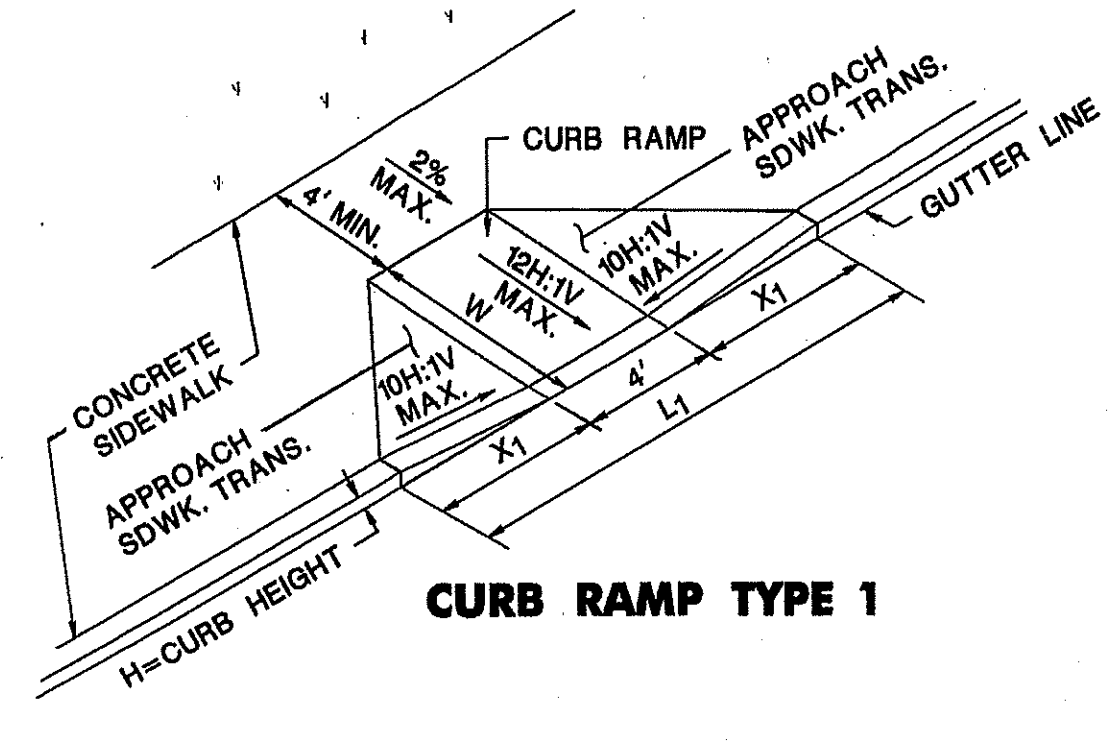
Harrington, Inc.  
2630 W. 21st St.  
Erie, PA 16506  
Ph. 800-553-0078  
fax. 814-838-7339  
daveh@harrinc.com

DWG No.:	HIHS-MLR-50-45		
Description:	5" Harrington Integral Hydrant Storz w/Cap for a Mueller Fire Hydrant		
Date:	04/08/10	DB: DTH	Rev.: 00
Scale:	NTS	Sheet 1 of 1	

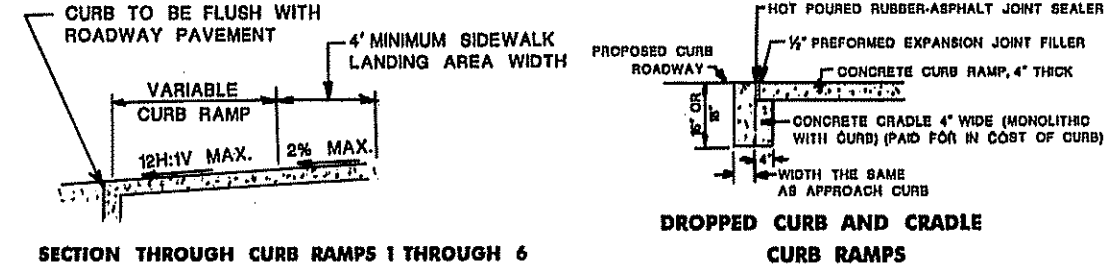
TOWN OF DOVER ENGINEERING DEPARTMENT		CONSTRUCTION DETAILS	
37 NORTH SUSSEX STREET DOVER, NEW JERSEY 07801		CONSTRUCTION PLANS FOR N. & S. BERGEN STREET ROADWAY IMPROVEMENTS TOWN OF DOVER MORRIS COUNTY, NEW JERSEY	
TEL. (973) 366-2200 ext. 154 / 152 FAX (973) 366-0039 Michael A. Hantson, PE, PP, CME - Town Engineer William J. Isselin - Assistant		PROJECT NO. 2018-01 SCALE: N.T.S.	
MICHAEL A. HANTSON - P.E. N.J. PROFESSIONAL ENGINEER LICENSE NO. 27078		SHEET 4 OF 6 REVISION:	

DATE:	REVISION:

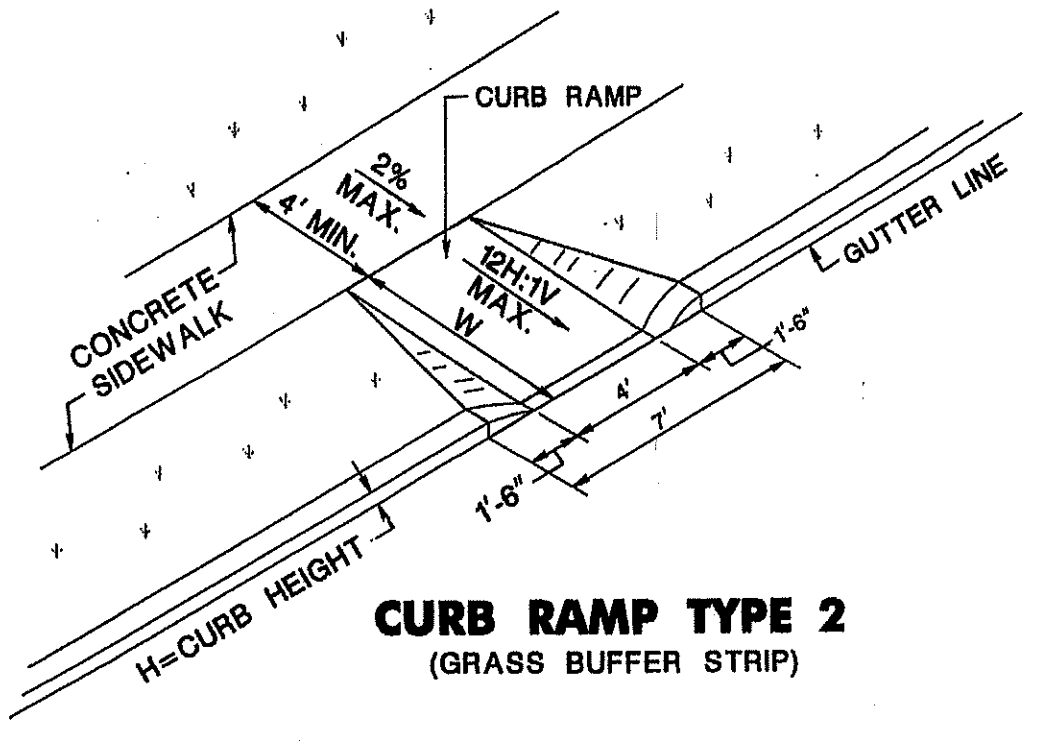
UNOFFICIAL COPY  
Not to Be Used For Bidding



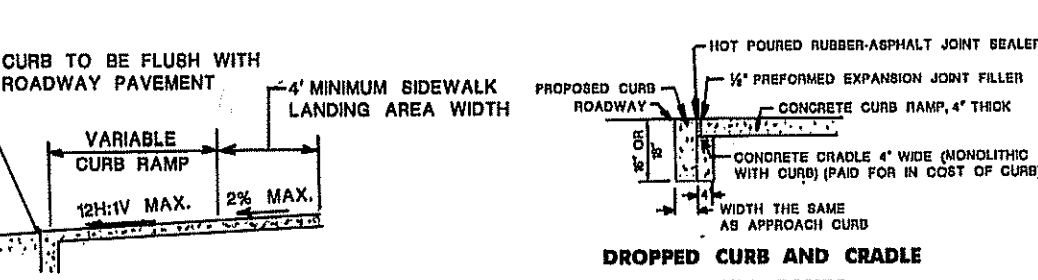
**CURB RAMP TYPE 1**



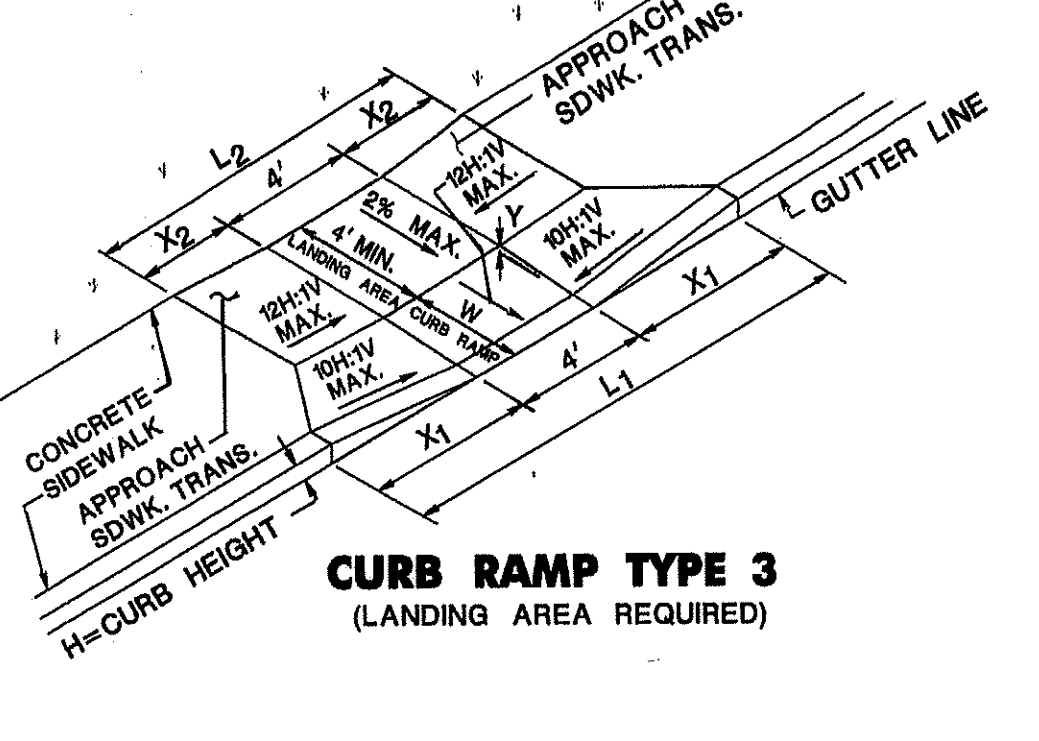
H INCHES	X1 FEET	L1 FEET	W FEET
3	2.5	9.0	3
4	3.3	10.6	4
5	4.2	12.4	5
6	5.0	14.0	6
7	5.8	15.6	7
8	6.7	17.4	8
9	7.5	19.0	9



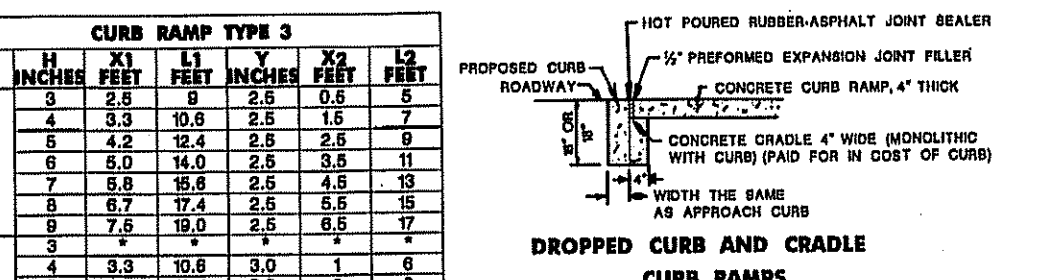
**CURB RAMP TYPE 2 (GRASS BUFFER STRIP)**



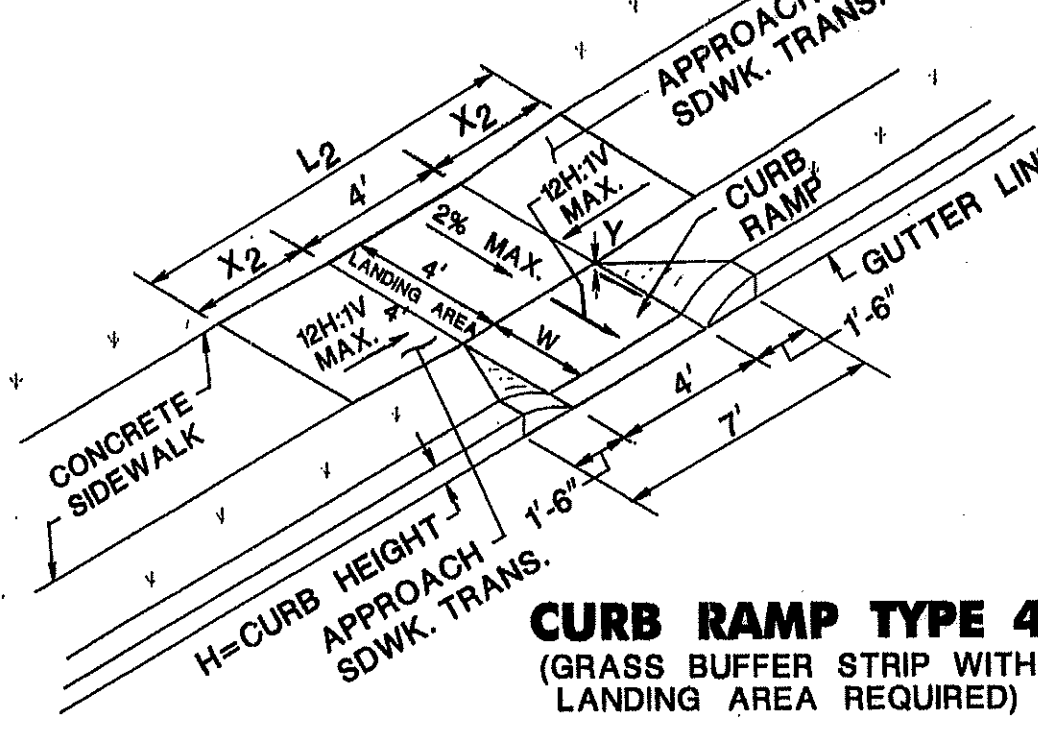
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



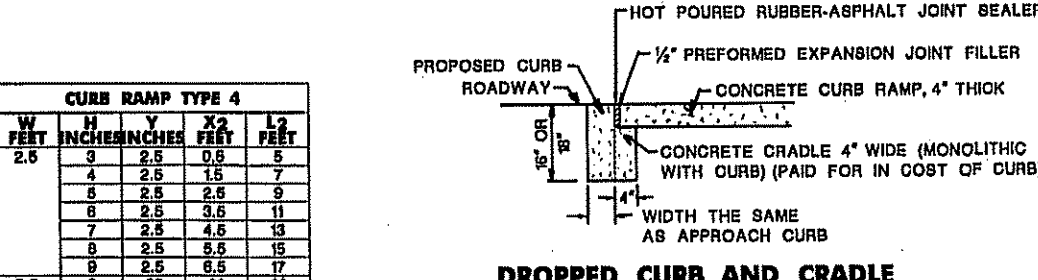
**CURB RAMP TYPE 3 (LANDING AREA REQUIRED)**



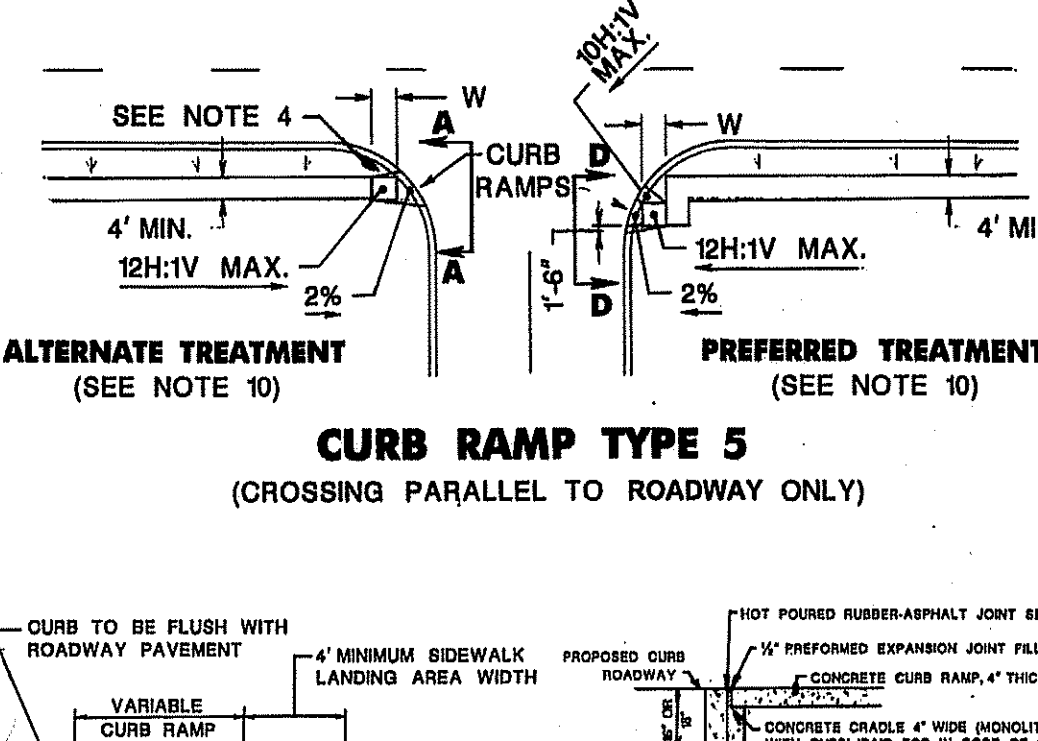
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



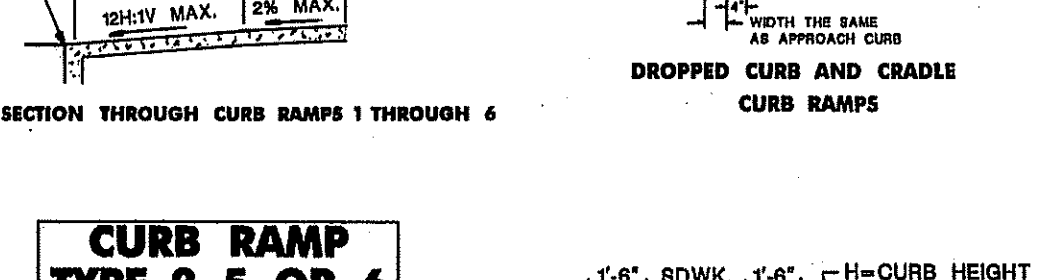
**CURB RAMP TYPE 4 (GRASS BUFFER STRIP WITH LANDING AREA REQUIRED)**



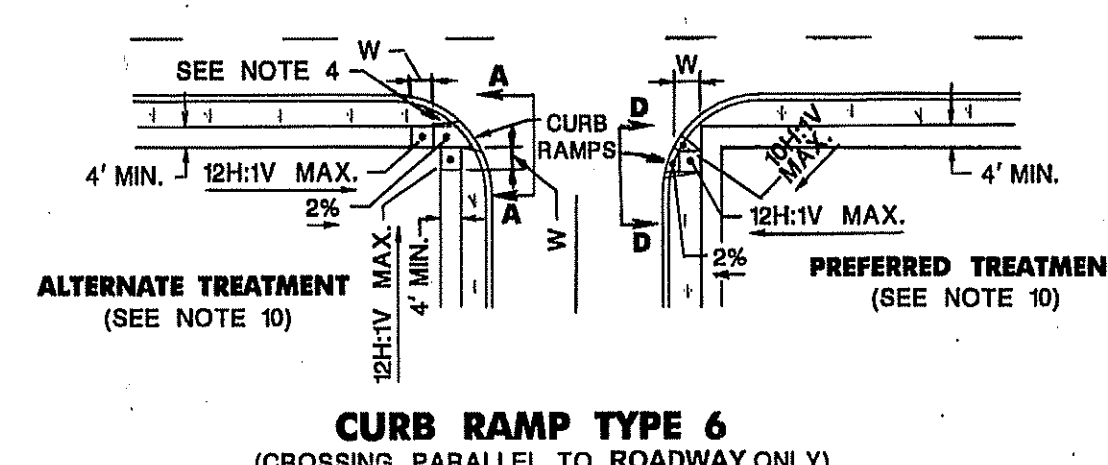
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



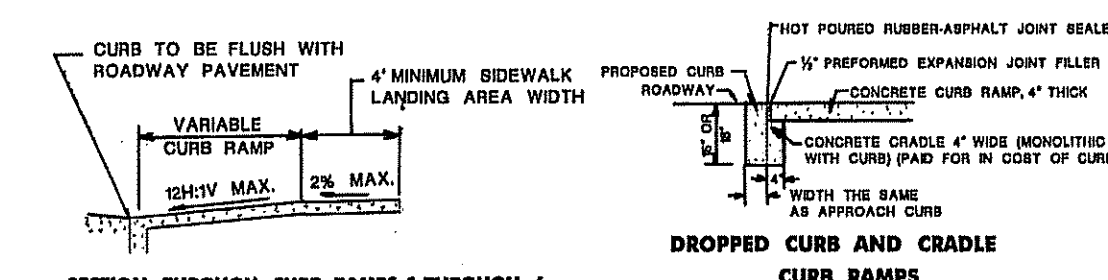
**CURB RAMP TYPE 5 (CROSSING PARALLEL TO ROADWAY ONLY)**



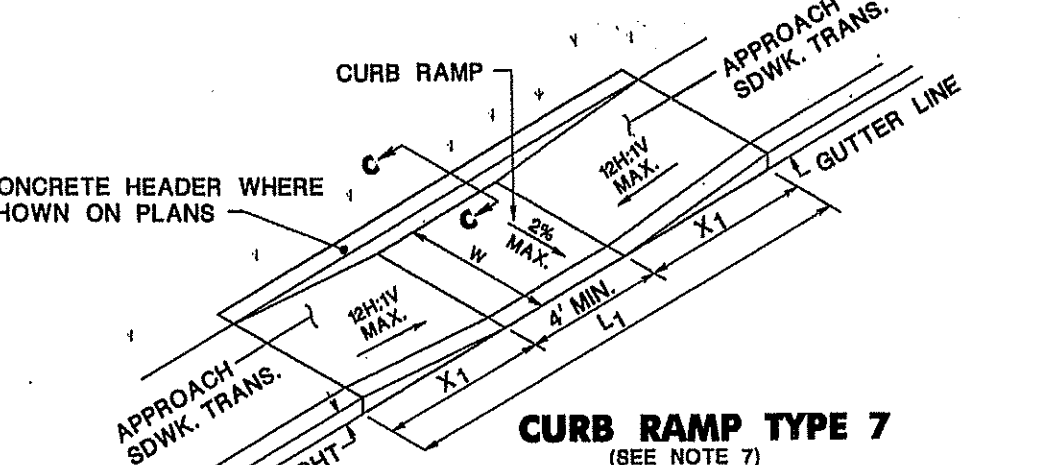
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



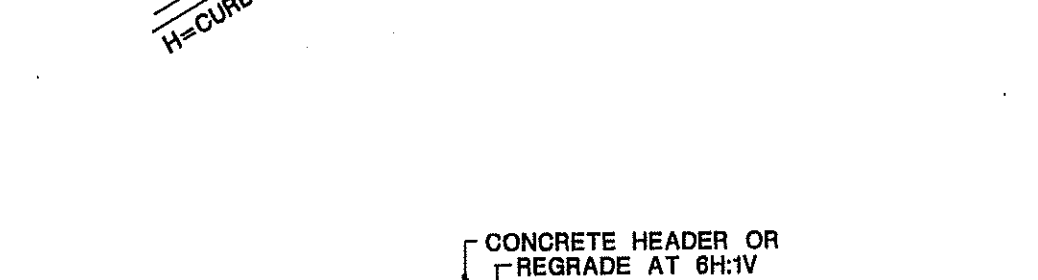
**CURB RAMP TYPE 6 (CROSSING PARALLEL TO ROADWAY ONLY)**



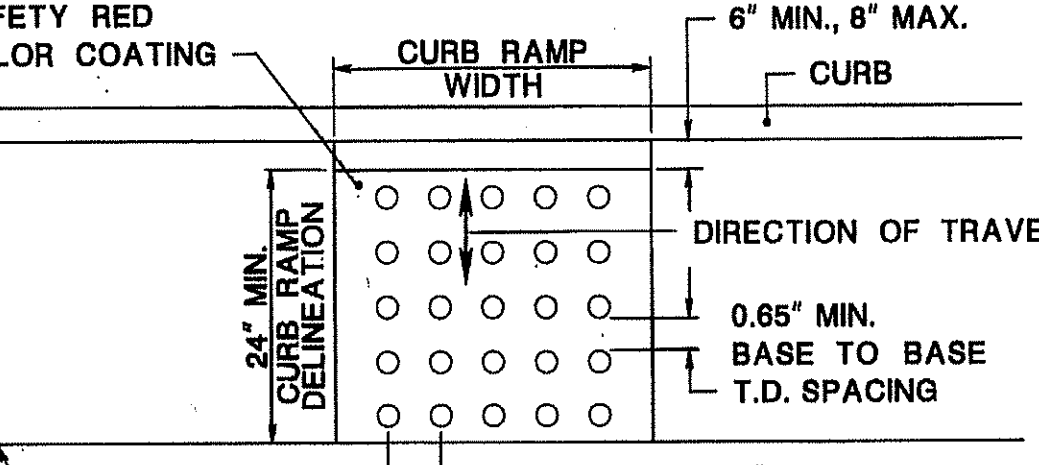
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



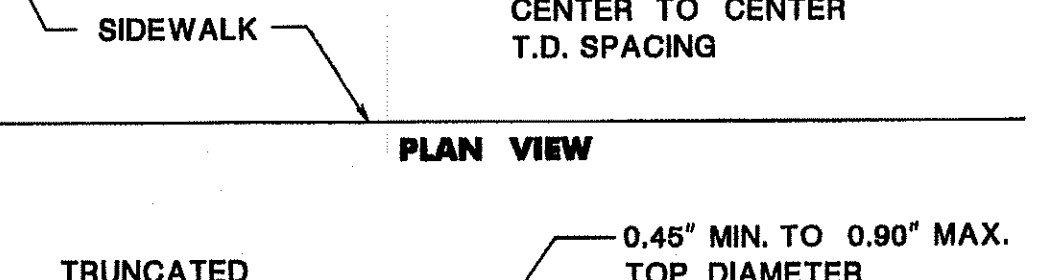
**CURB RAMP TYPE 7 (SEE NOTE 7)**



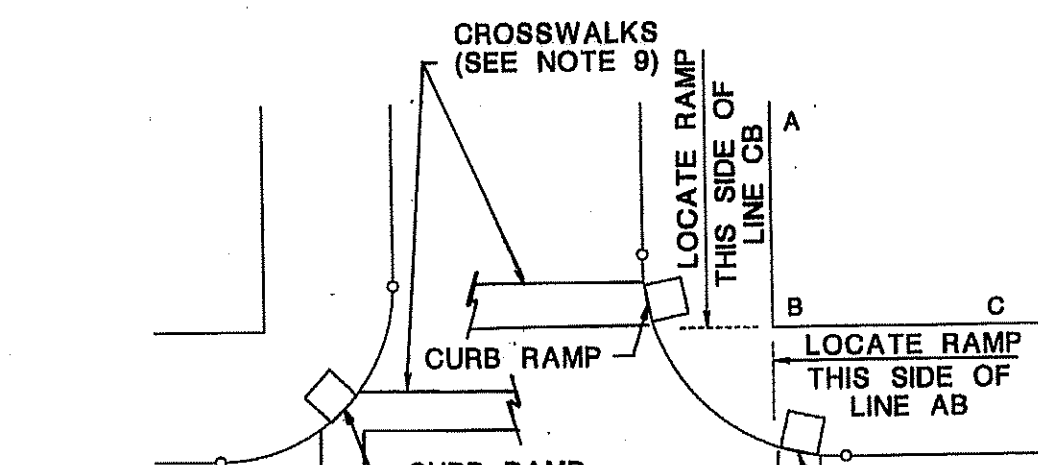
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



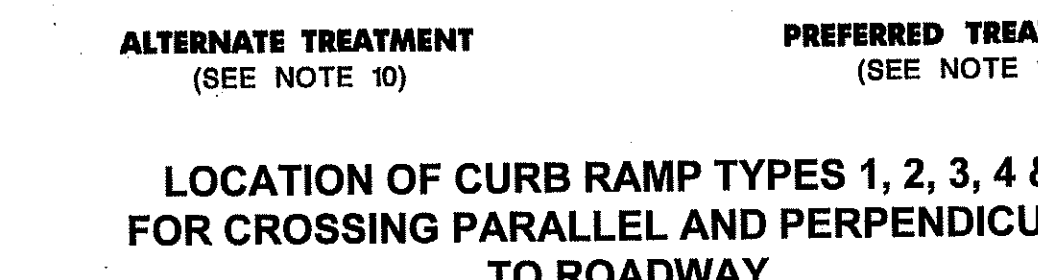
**DETECTABLE WARNING SURFACE**



H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9



**LOCATION OF CURB RAMP TYPES 1, 2, 3, 4 & 7 FOR CROSSING PARALLEL AND PERPENDICULAR TO ROADWAY**



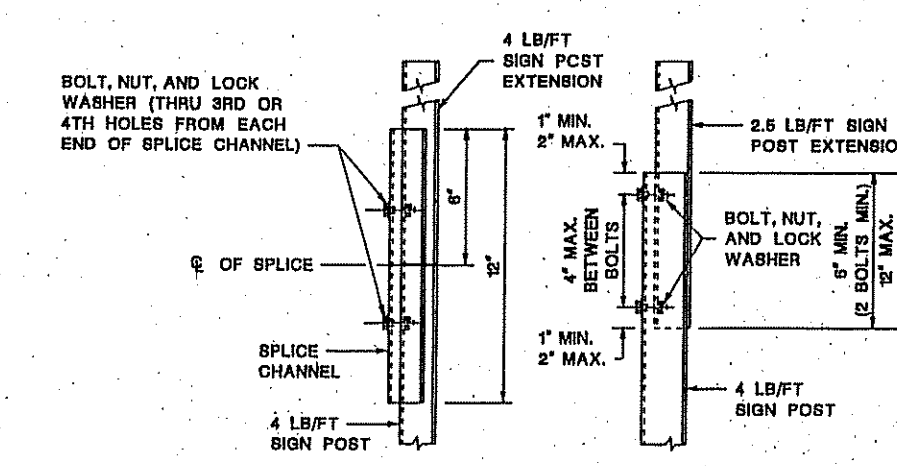
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9

- GENERAL NOTES:**
- LANDING AREA, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP SHALL BE KEPT CLEAR OF OBSTRUCTIONS.
  - DIMENSIONS SHOWN IN TABLES ARE FOR RELATIVELY FLAT SIDEWALK AREAS. CARE SHOULD BE TAKEN WHEN DETERMINING CURB RAMP SIZE BASED ON CURB HEIGHT (H) WHERE ELEVATION OF CURB AND SIDEWALK VARY DRASTICALLY IN AREA OF PROPOSED CURB RAMP.
  - CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY PAVEMENT A MINIMUM OF 4 FEET AT ALL CURB RAMP.
  - FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
  - SIDEWALK AND CURB RAMP WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
  - CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS VERTICAL CURB OR SLOPING CURB OF THE APPROPRIATE ADJACENT SIZE AND KIND.
  - WHERE THE DISTANCE FROM THE GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 6 FEET OR LESS, CURB RAMP TYPE 7 SHOULD BE USED, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4.
  - THE PUBLIC SIDEWALK CURB RAMP, DETECTABLE WARNING SURFACE (SHADED AREA) SHALL BE SAFETY RED COLOR ON CONCRETE OR 70% COLOR CONTRAST FOR OTHER SURFACE SUCH AS BRICK.
  - CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED, SEE PLANS.
  - PREFERRED AND ALTERNATE TREATMENTS SHOULD NOT BE INTERMIXED WITHIN THE SAME INTERSECTION.
  - DIMENSIONS SHOWN IN TABLES ARE FOR 3 INCH TO 9 INCH CURB HEIGHTS. WHERE THE CURB HEIGHTS ARE OTHER THAN WHAT IS PROVIDED IN THE TABLES, THE DIMENSIONS OF THE RAMP WILL HAVE TO BE CALCULATED BASED ON CROSS SLOPES SHOWN.

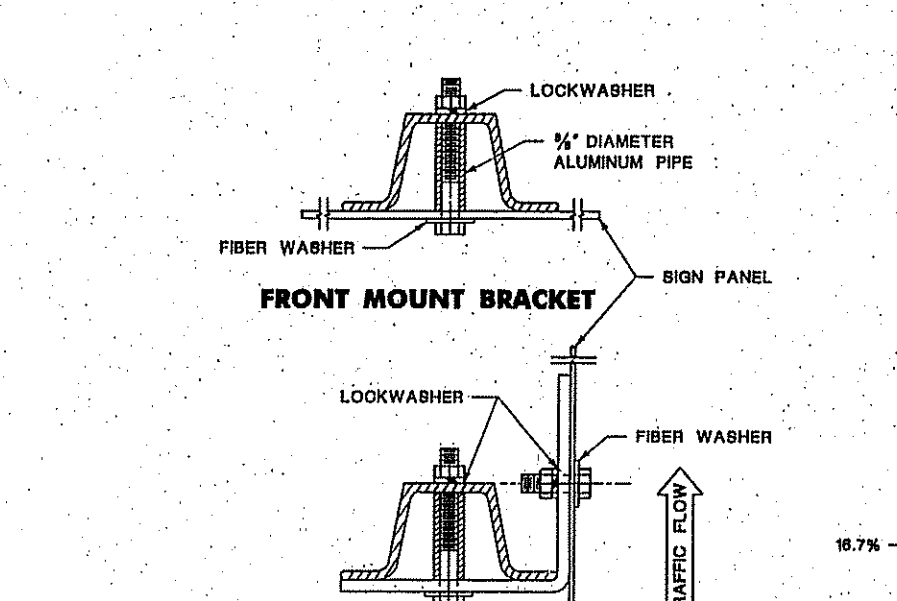
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<b>TOWN OF DOVER</b> <b>ENGINEERING DEPARTMENT</b> MAIL: 37 N. SUSSEX STREET OFFICE: 100 PRINCETON AVENUE DOVER, NEW JERSEY 07801 TEL. (973) 366-2200 ext. 2154/2152 FAX (973) 366-0039 Michael A. Hantson, PE, PP, CME - Town Engineer William J. Isselin - Assistant		<b>HANDICAP RAMP</b> <b>DETAILS</b> CONSTRUCTION PLANS FOR N. & S. BERGEN STREET ROADWAY IMPROVEMENTS TOWN OF DOVER MORRIS COUNTY, NEW JERSEY	
MICHAEL A. HANTSON - P.E. N.J. PROFESSIONAL ENGINEER LICENSE NO. 27078		PROJECT NO. 2018-01 SCALE: N.T.S. SHEET 5 OF 6 REVISION:	

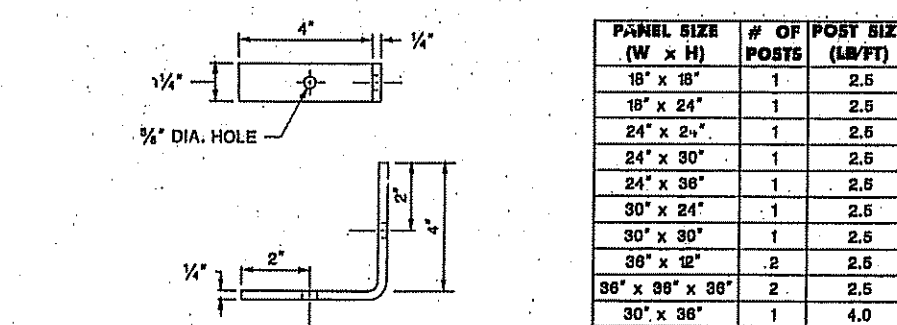




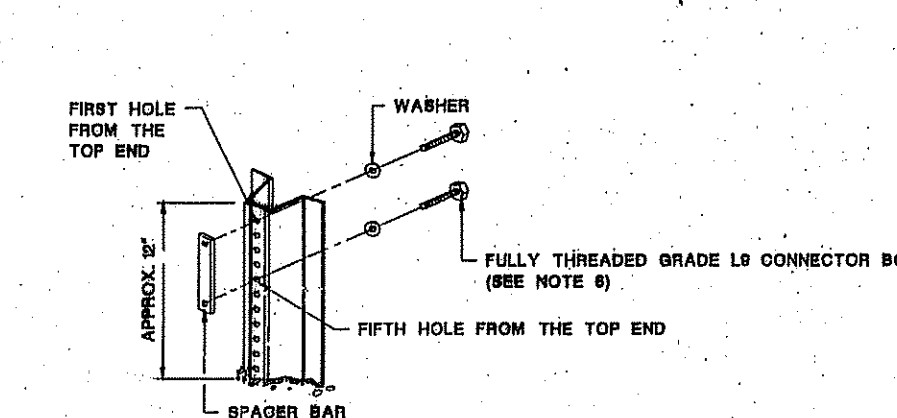
**SIGN POST EXTENSION  
SPICE DETAILS**



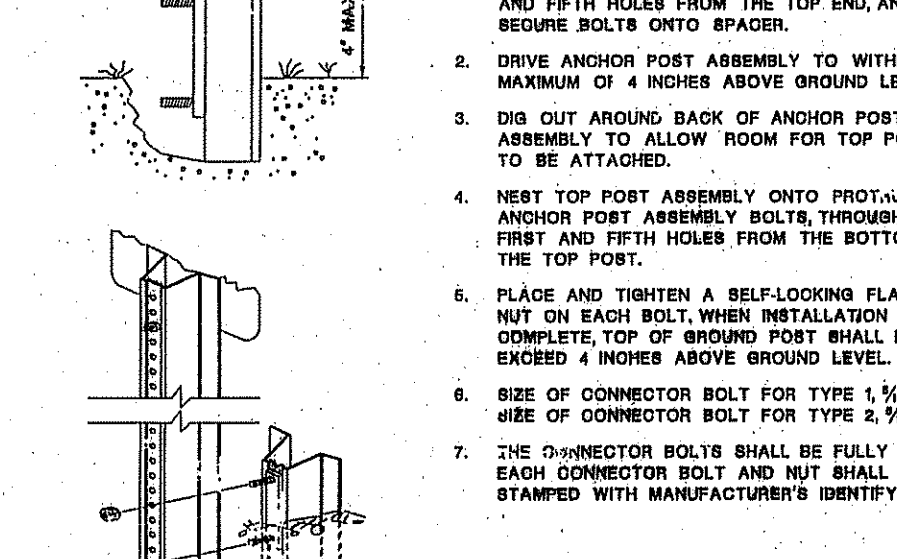
**FRONT MOUNT BRACKET**  
**SIDE MOUNT BRACKET**



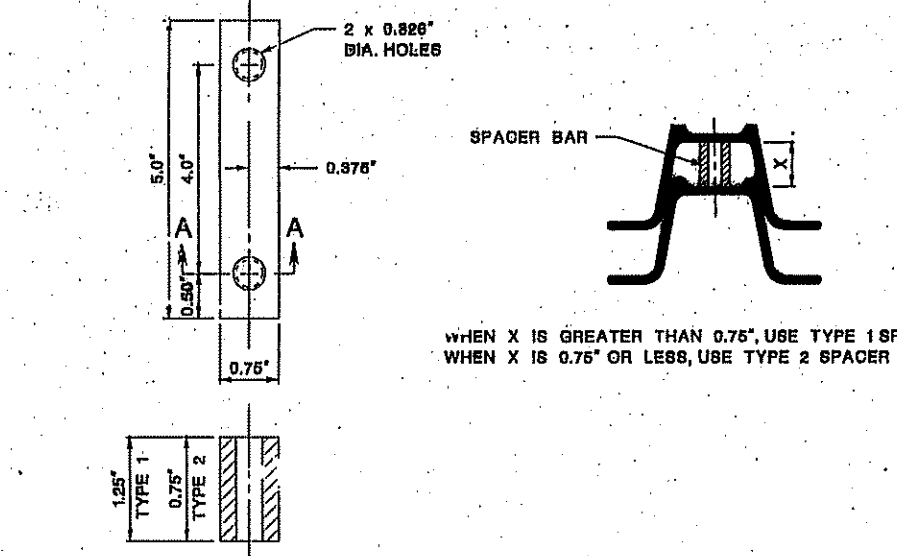
**DETAIL OF BRACKET  
FOR SIDE MOUNTED SIGNS**



**ANCHOR POST ASSEMBLY  
SIGN SUPPORTS**



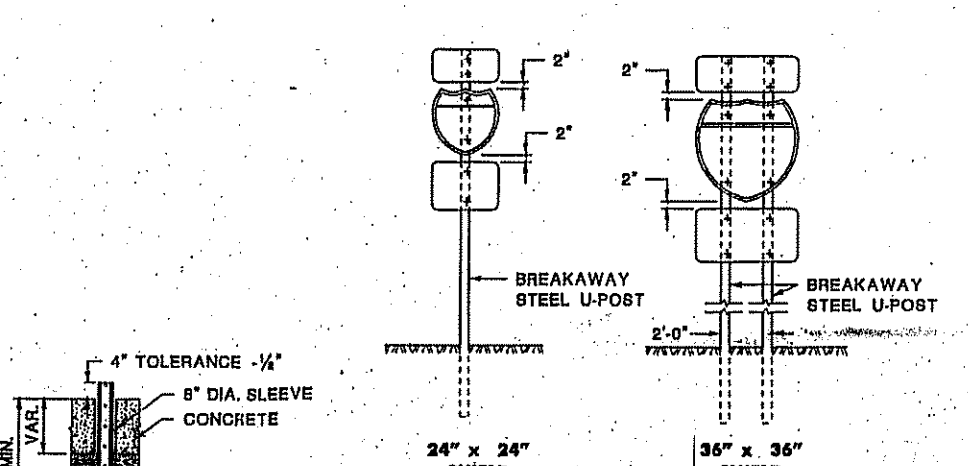
**ANCHOR POST ASSEMBLY  
SIGN SUPPORTS**



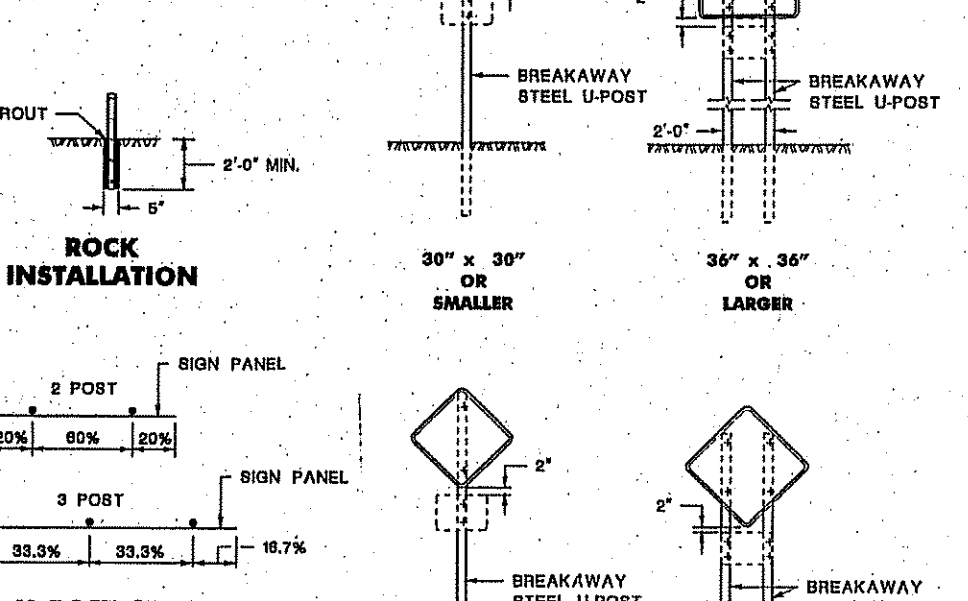
**ANCHOR POST ASSEMBLY  
SIGN SUPPORTS**



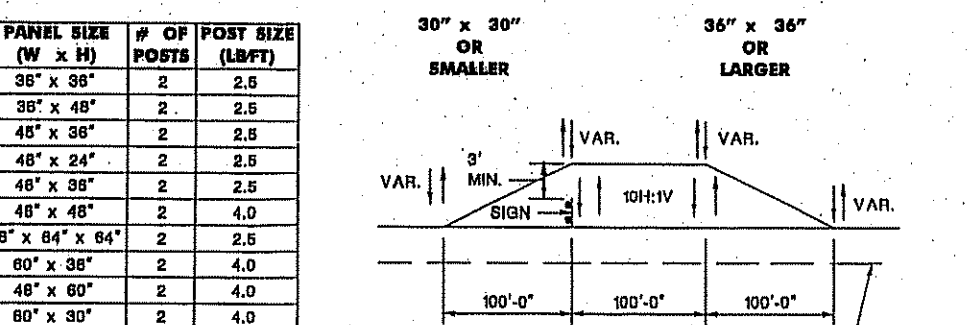
**ANCHOR POST ASSEMBLY  
SIGN SUPPORTS**



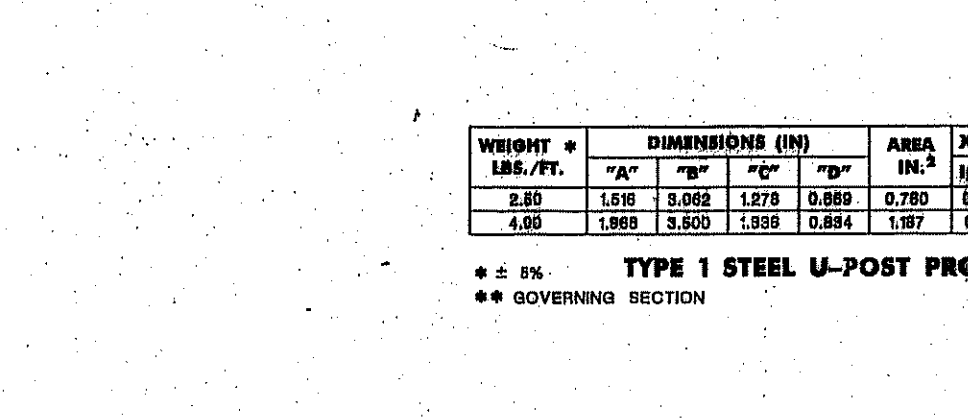
**CONCRETE  
INSTALLATION**



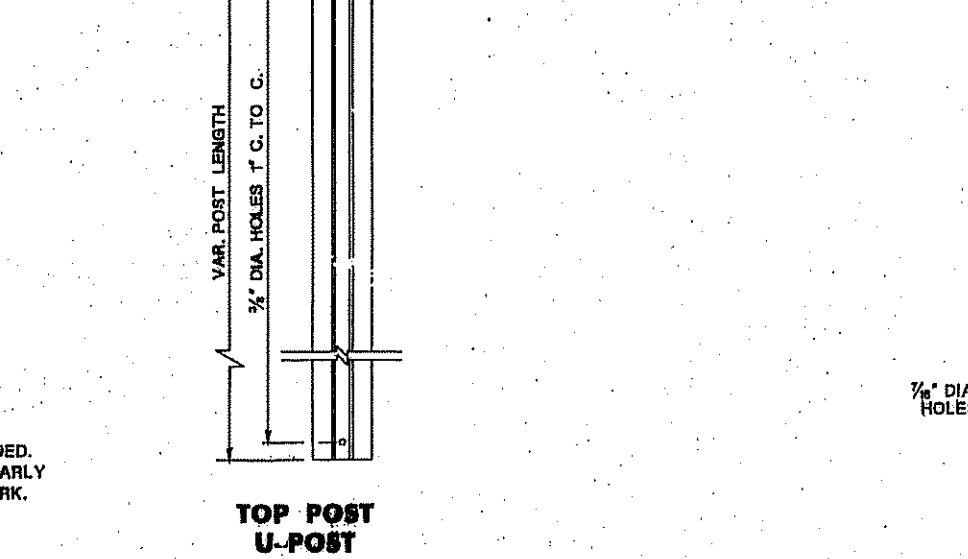
**ROCK  
INSTALLATION**



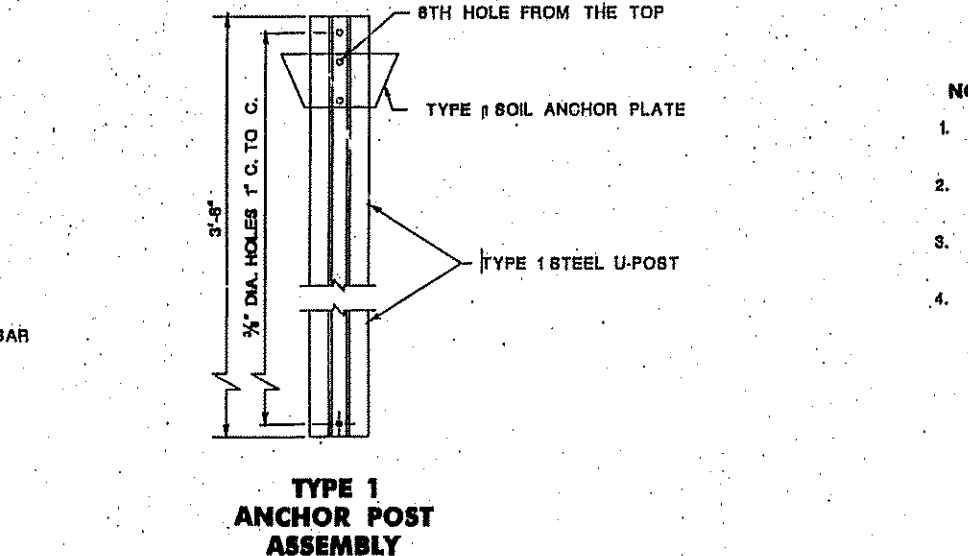
**STEEL U-POST SPACING**



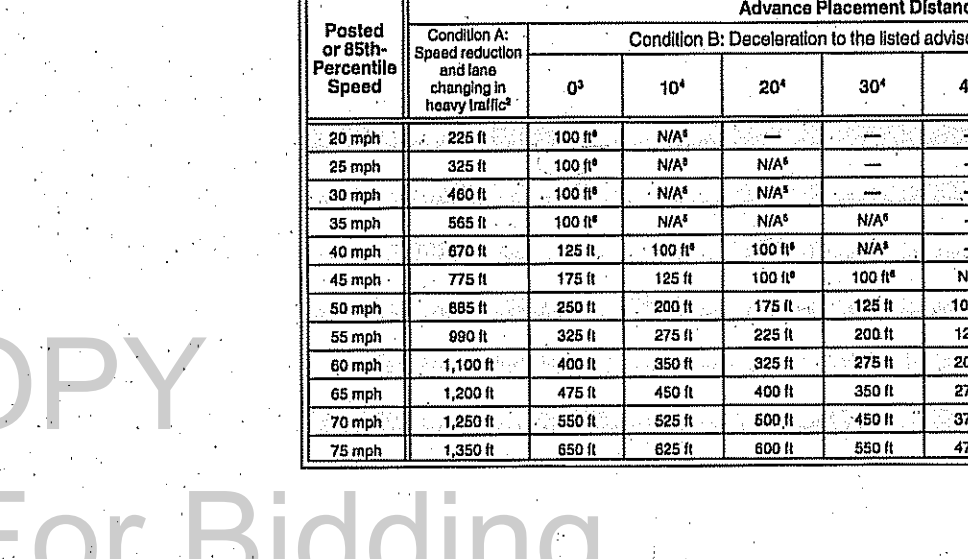
**STEEL U-POST GRADING  
DETAIL**



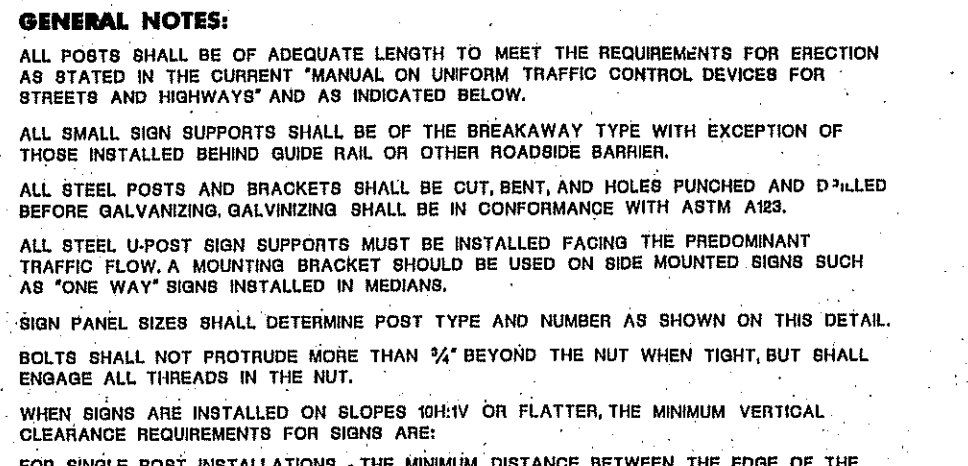
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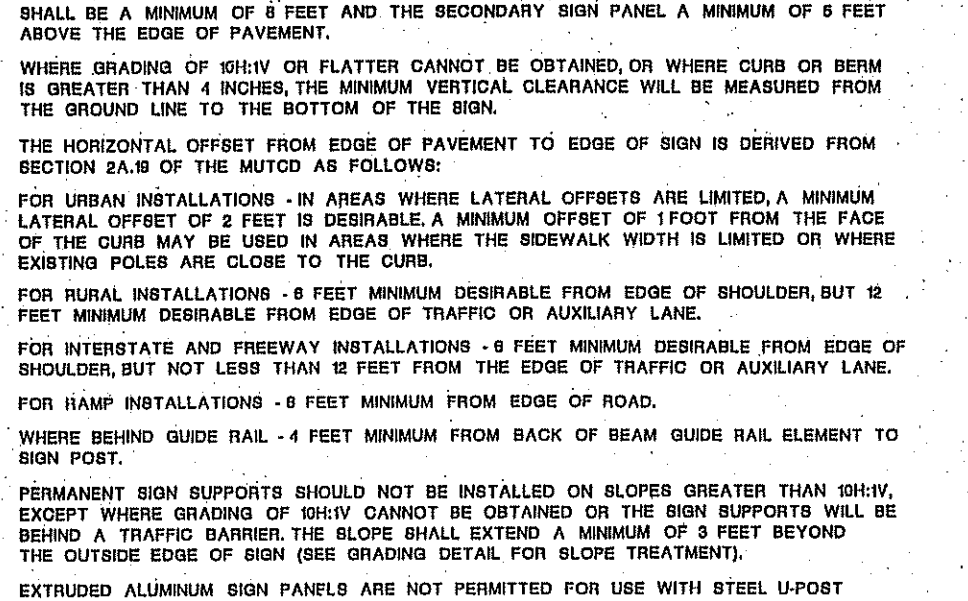
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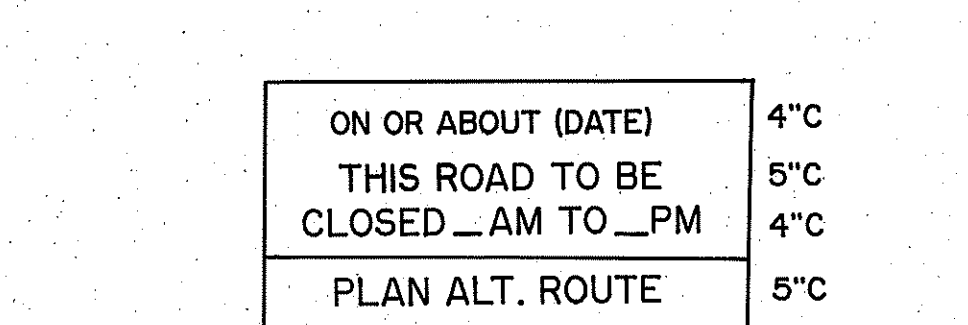
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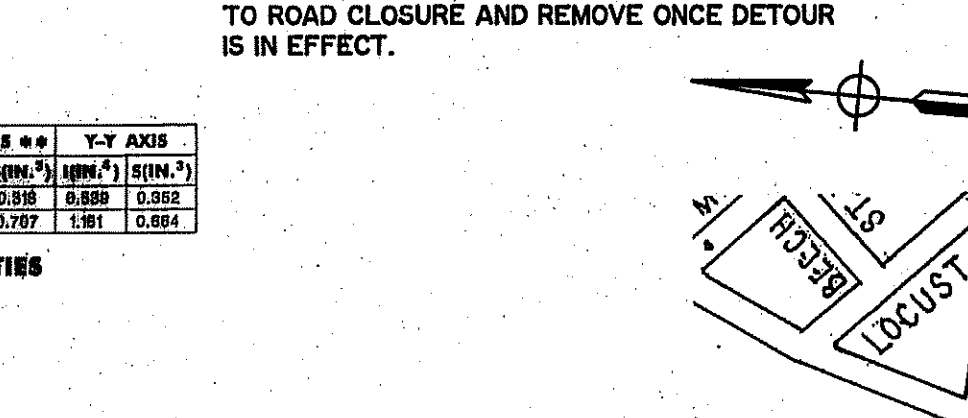
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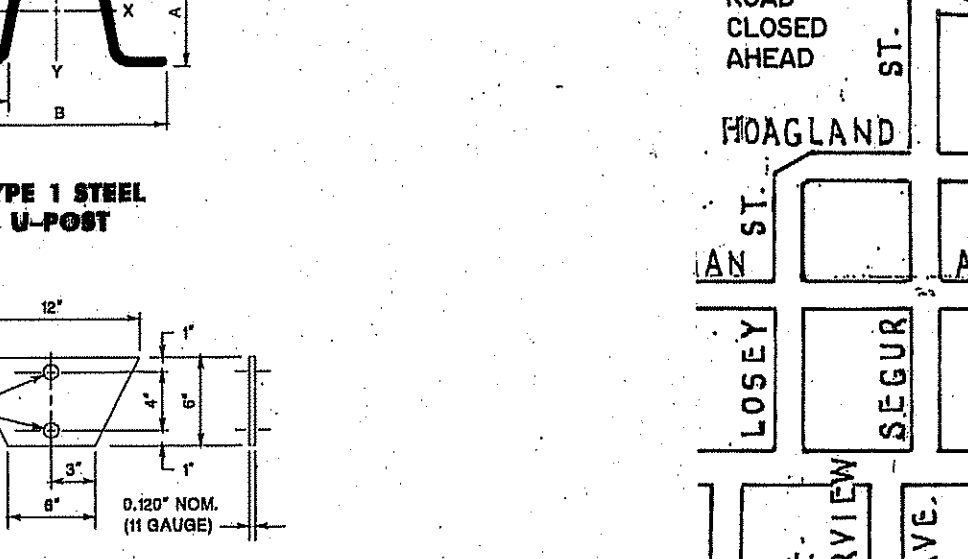
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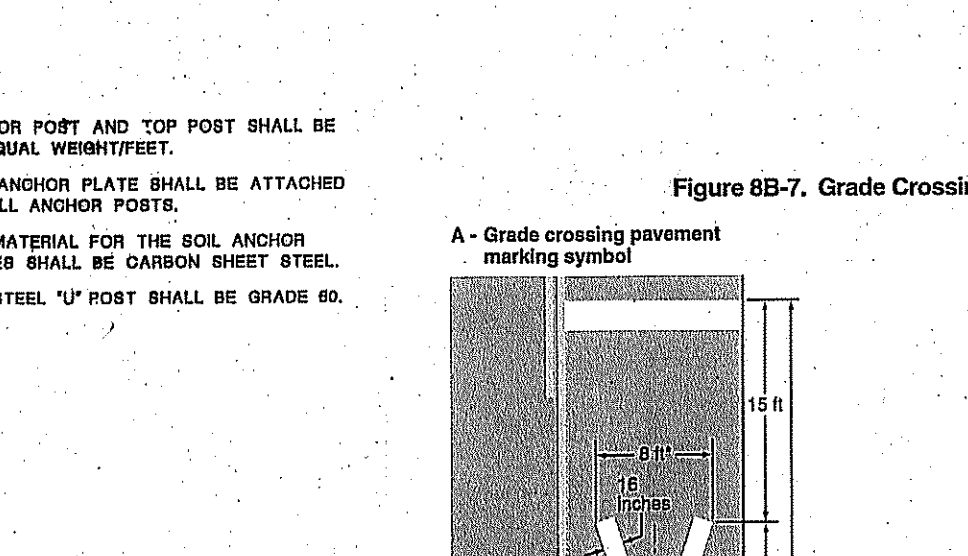
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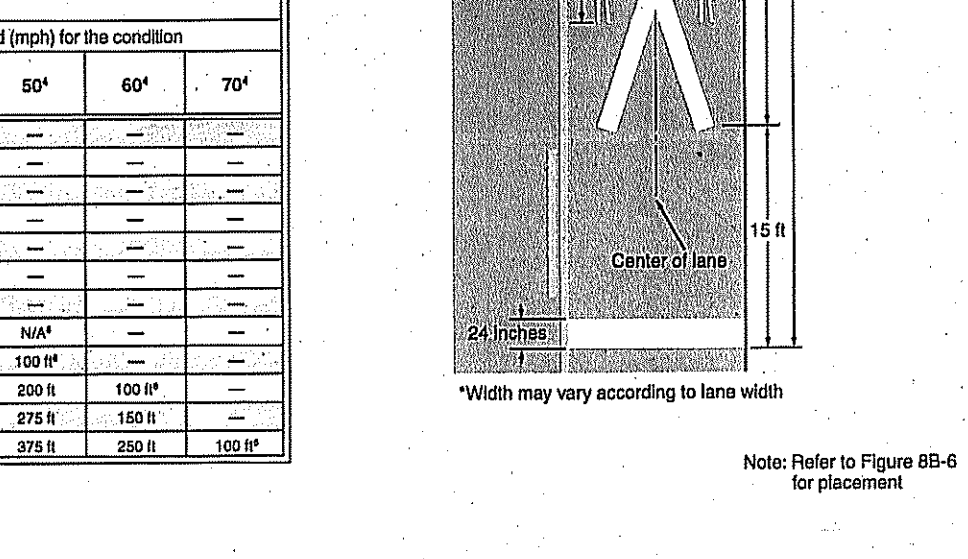
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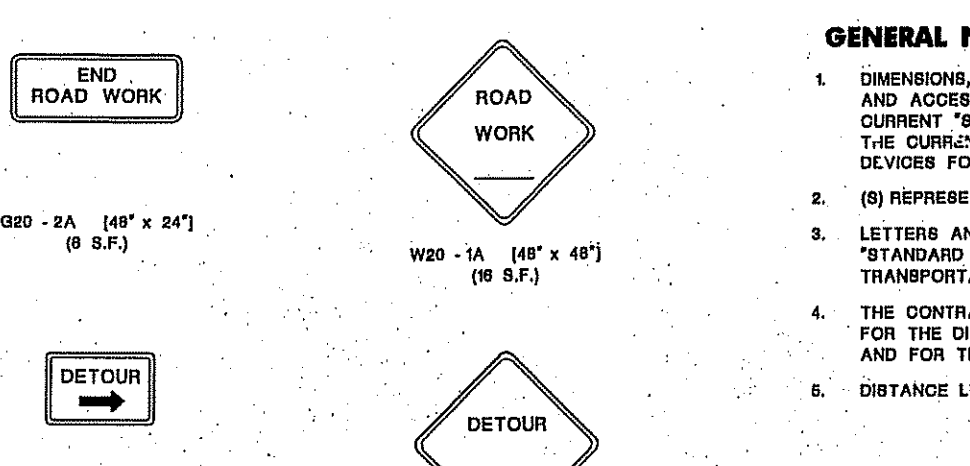
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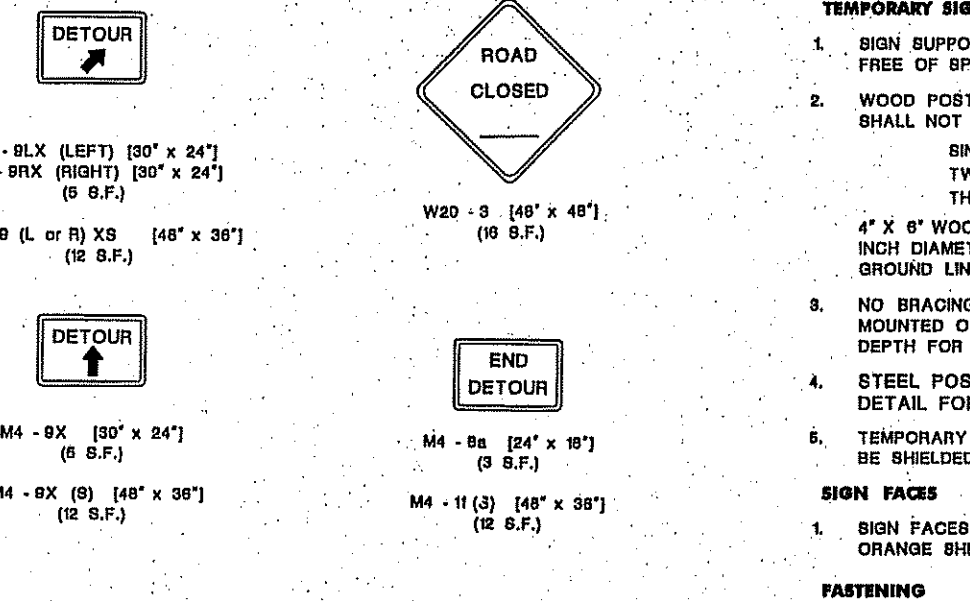
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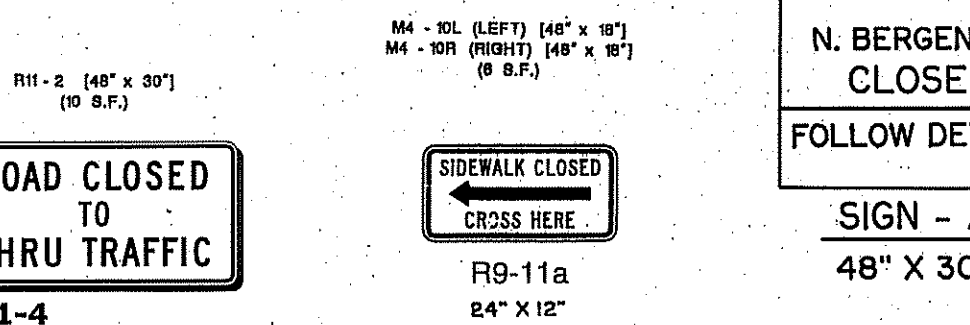
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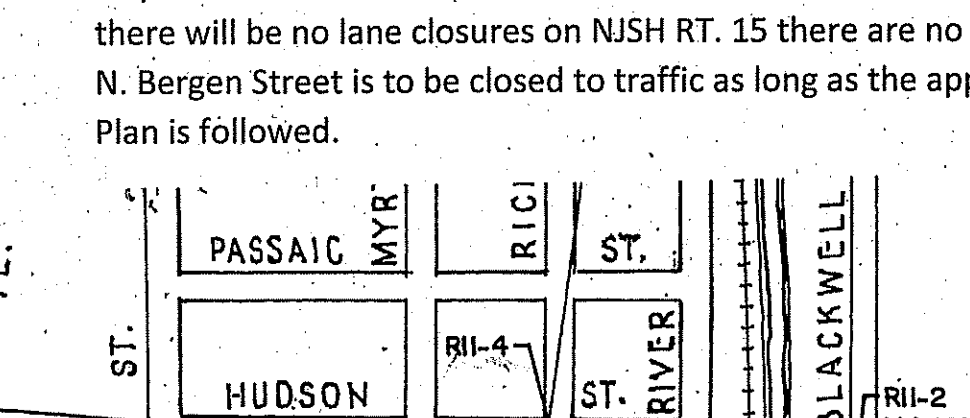
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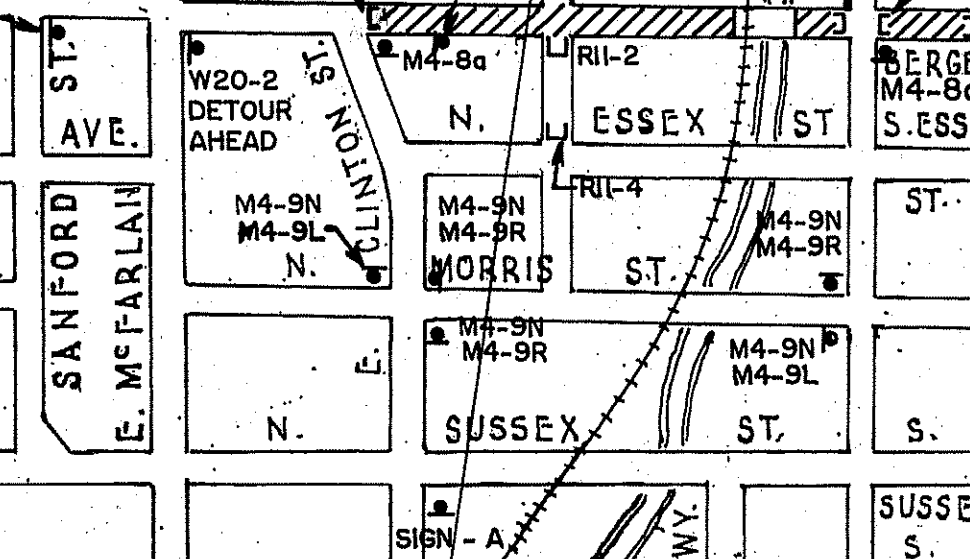
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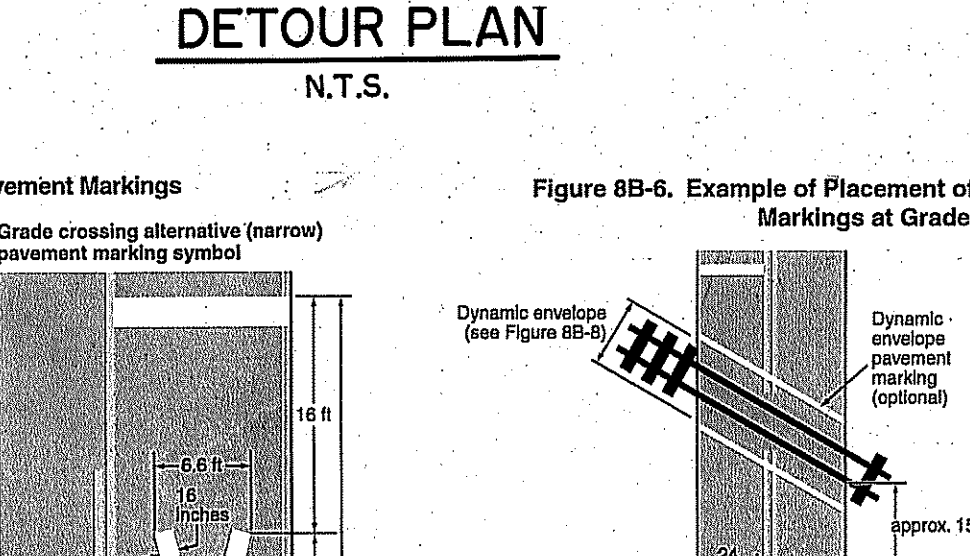
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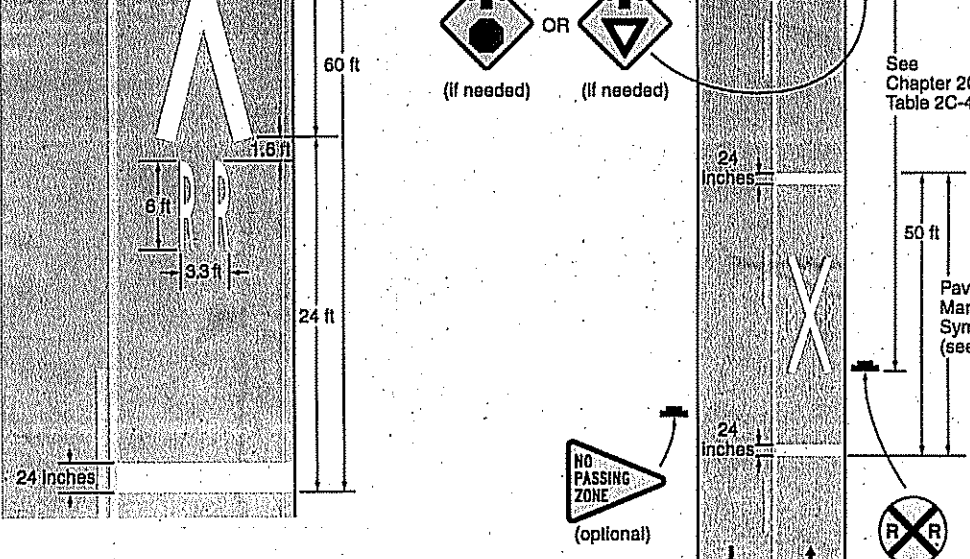
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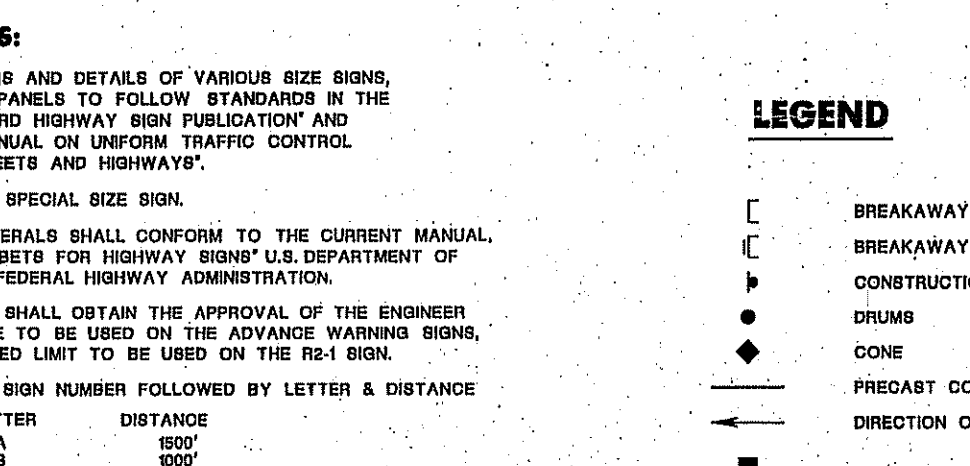
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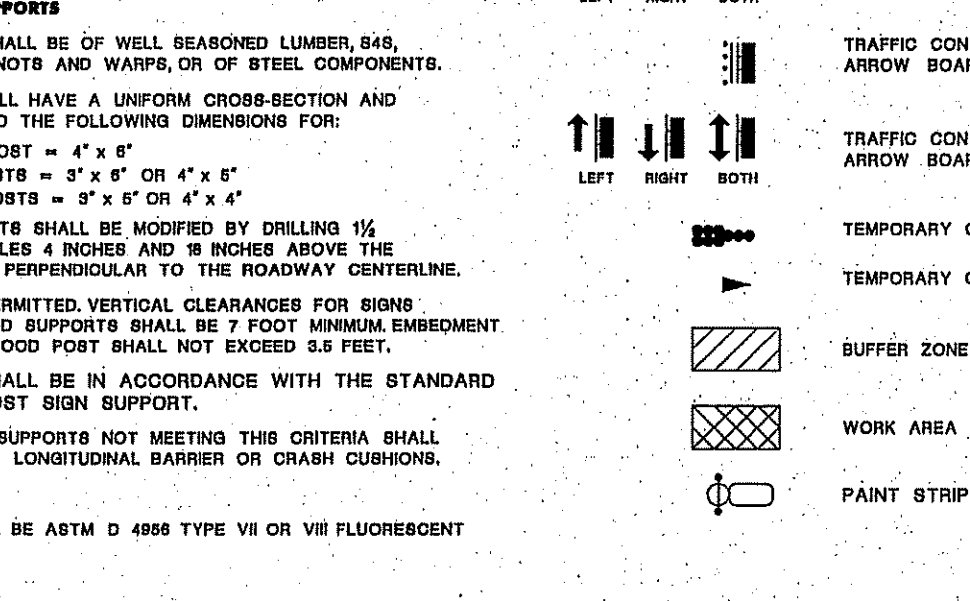
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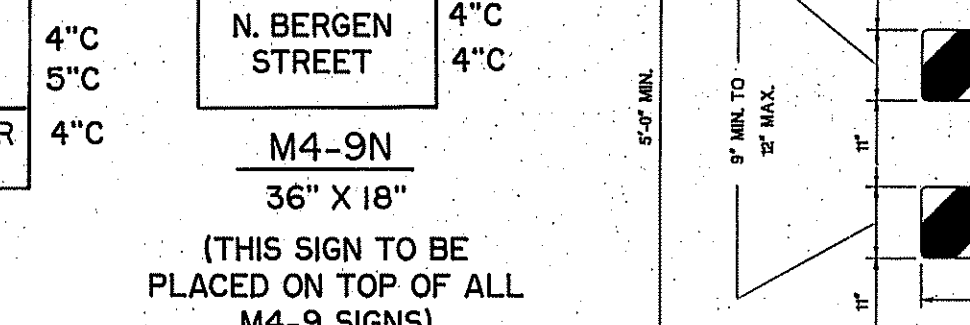
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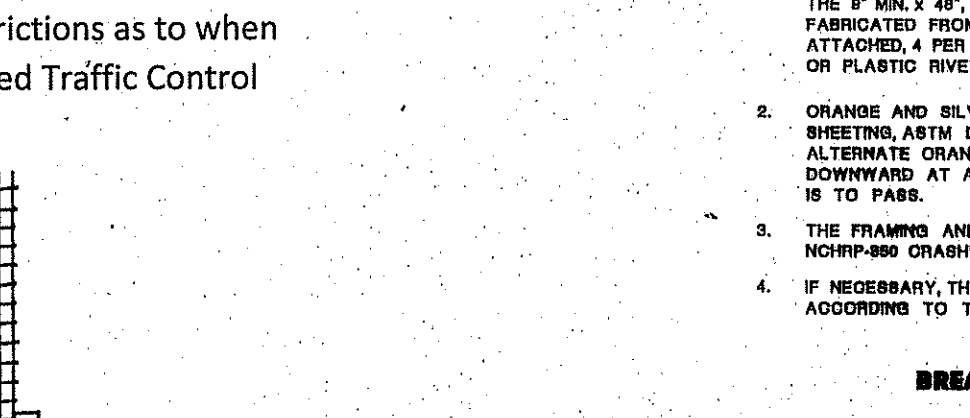
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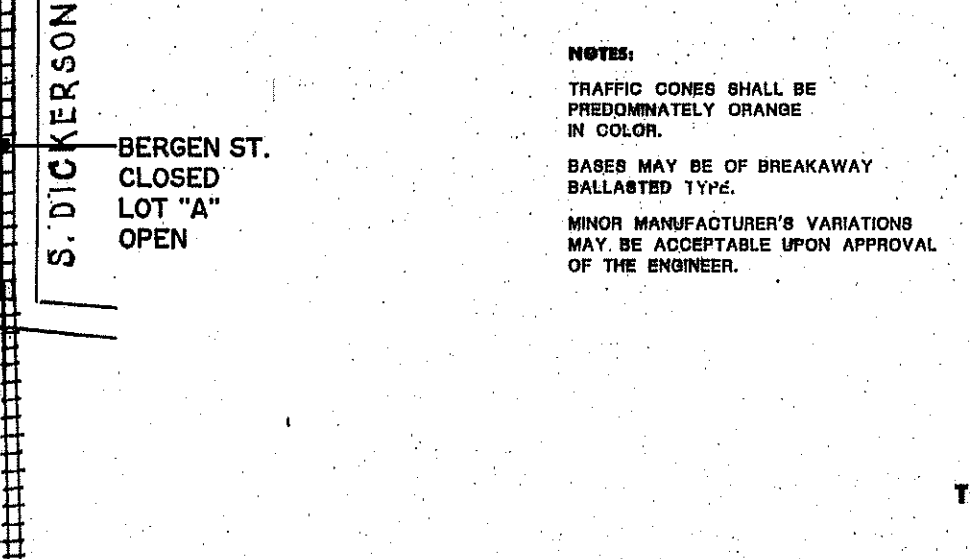
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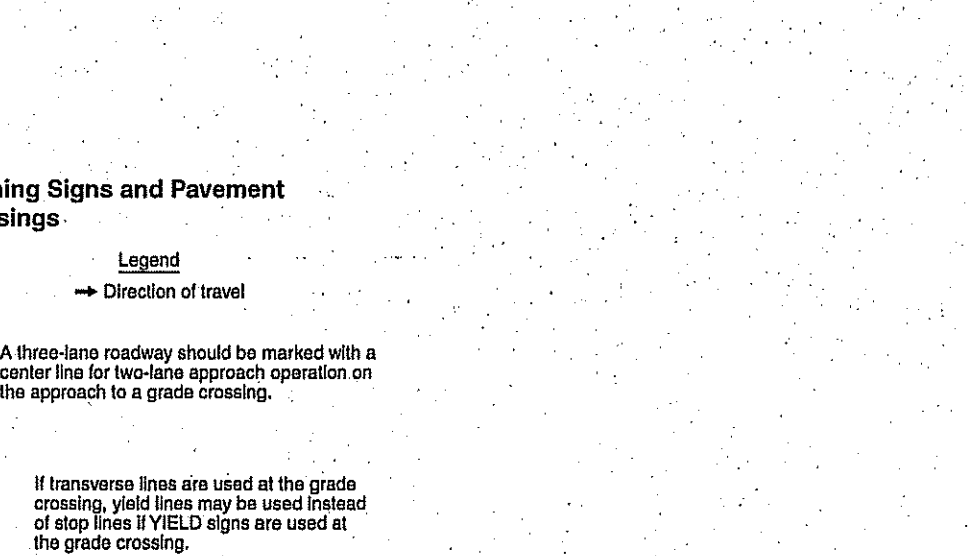
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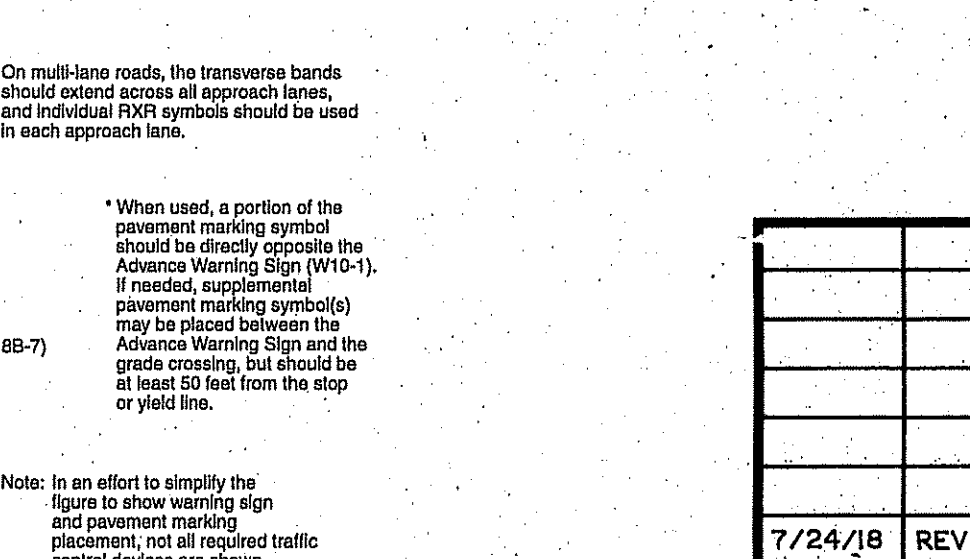
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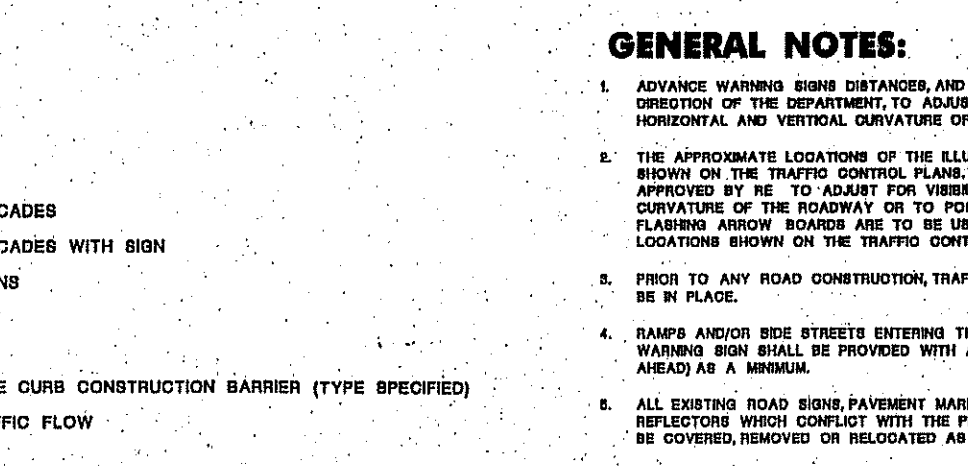
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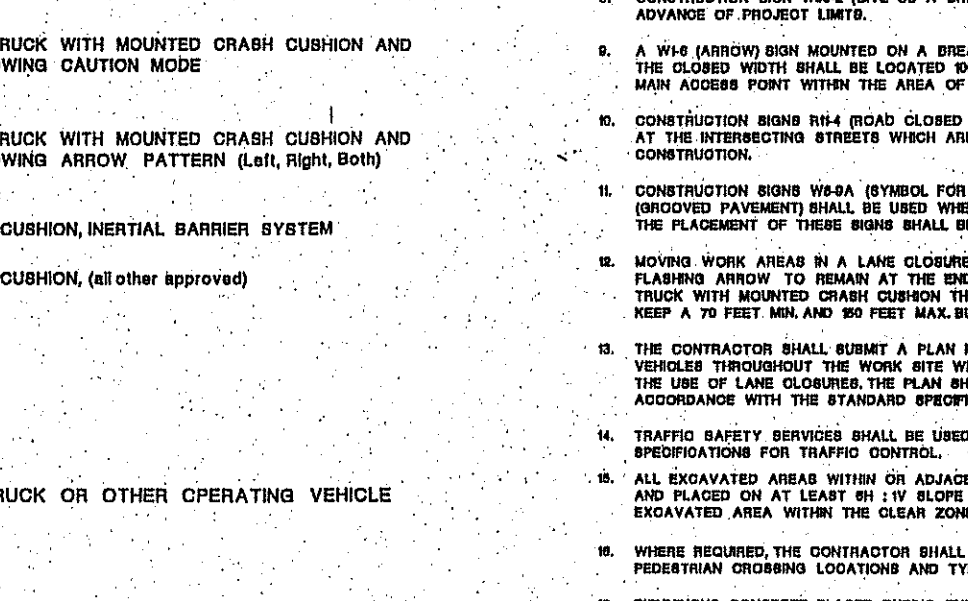
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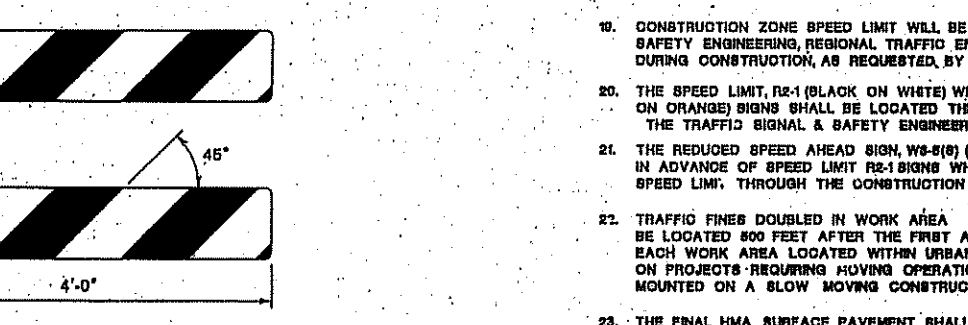
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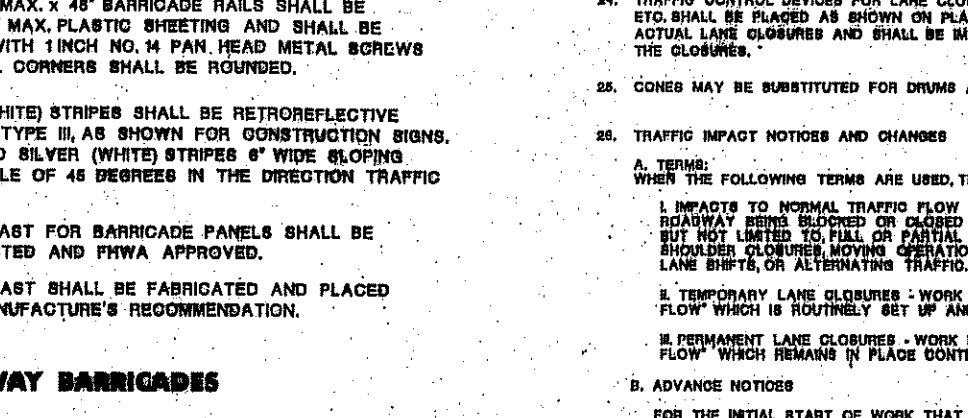
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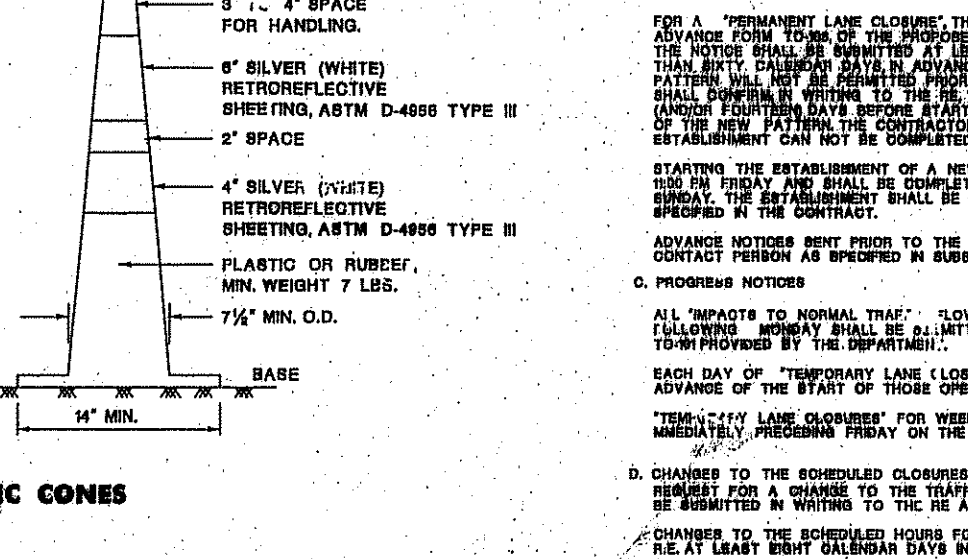
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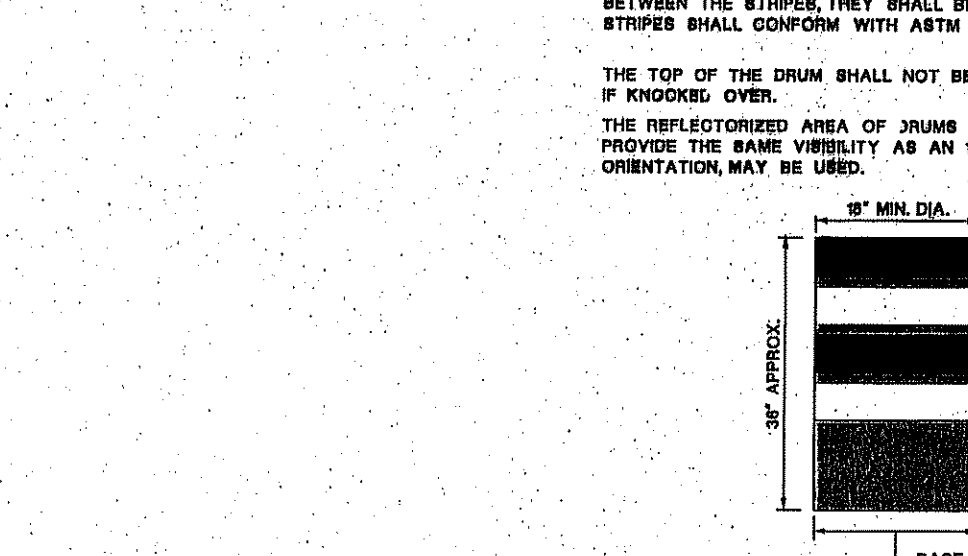
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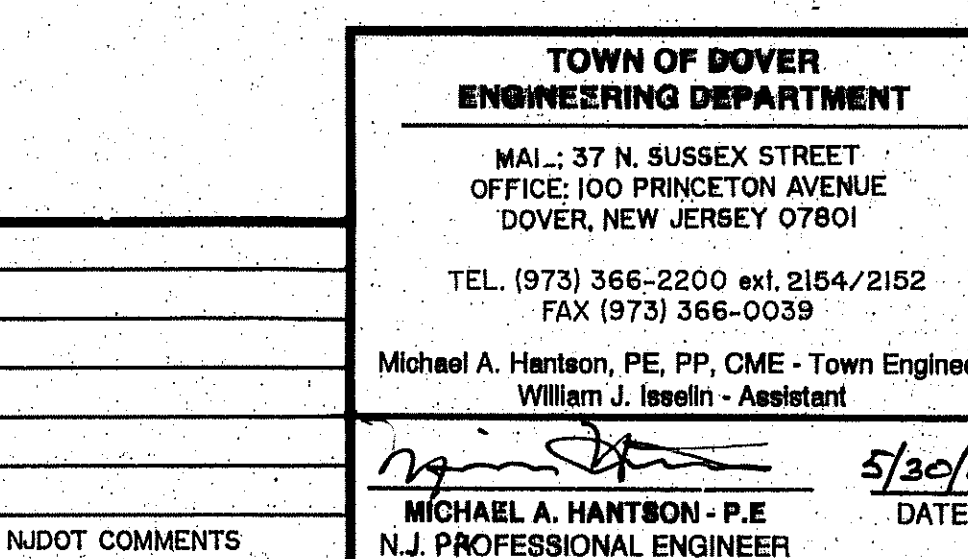
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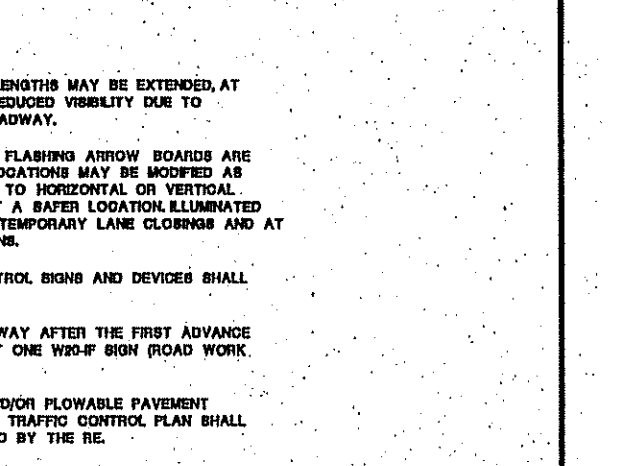
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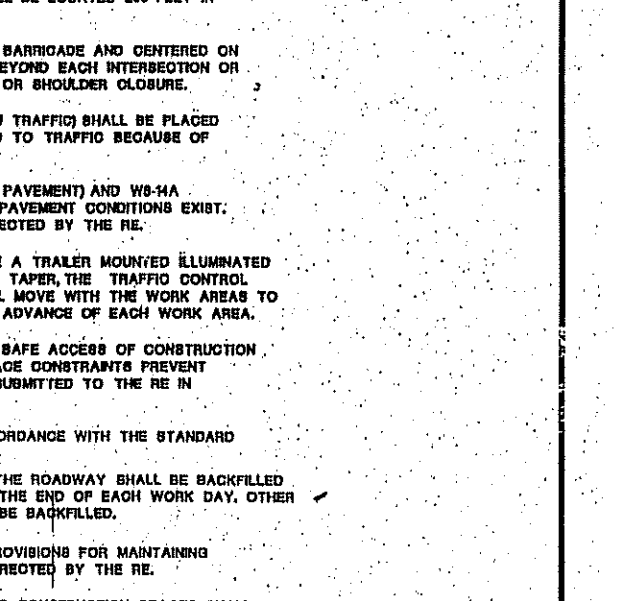
**STEEL U-POST PROPERTIES**



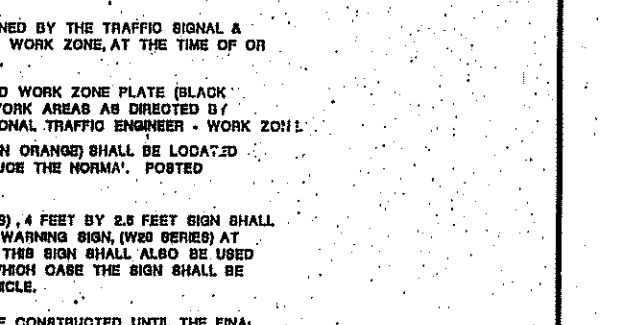
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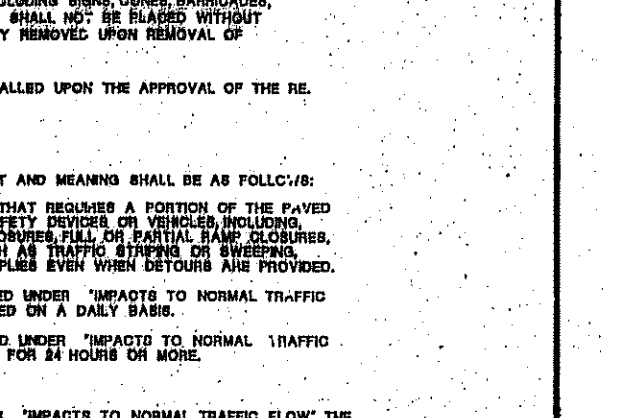
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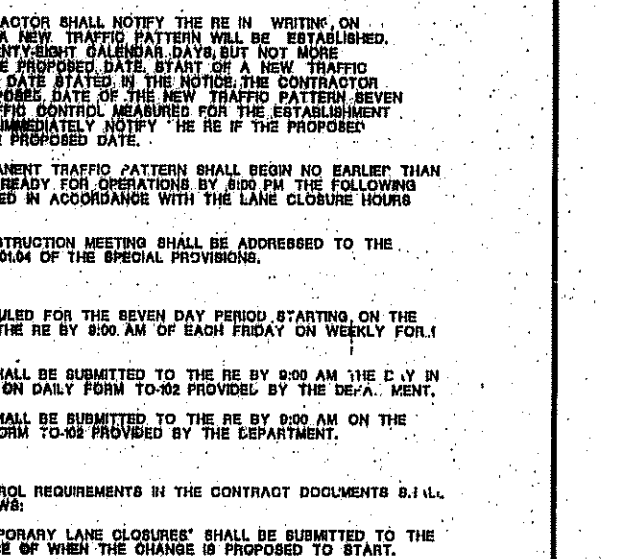
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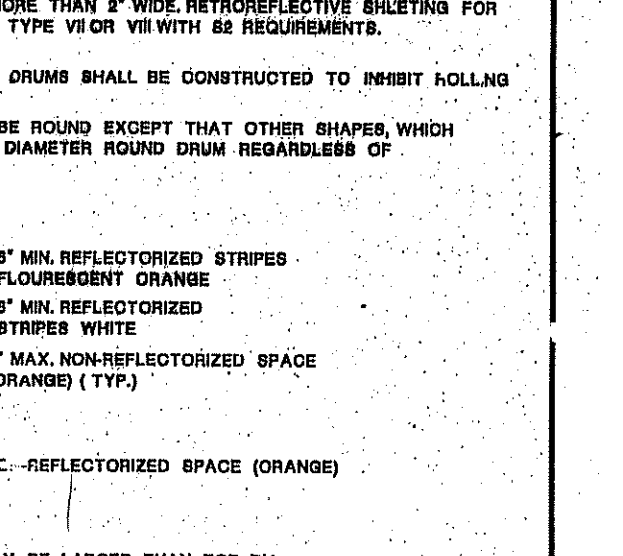
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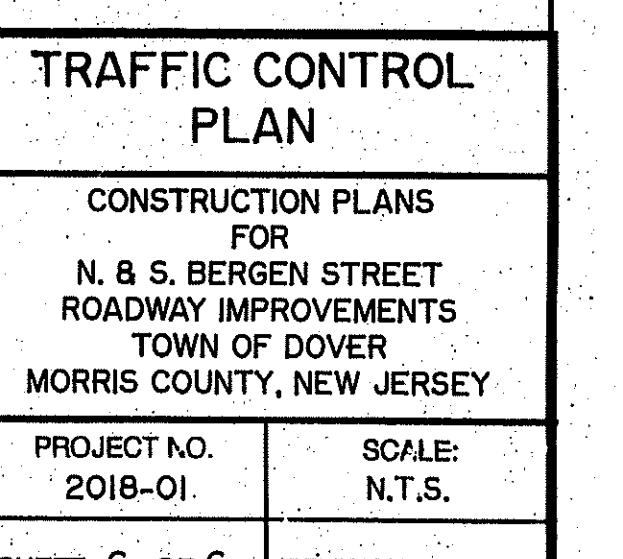
**STEEL U-POST PROPERTIES**



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Table 2C-4. Guidelines for Advance Placement of Warning Signs

Posted or Speed Limit (mph)	Condition A: Good weather and no change in traffic volume		Condition B: Decrease in the listed advisory speed (mph) for the condition	
	10'	20'	30'	40'
20 mph	225 ft	100 ft	N/A	N/A
25 mph	325 ft	100 ft	N/A	N/A
30 mph	425 ft	100 ft	N/A	N/A
35 mph	525 ft	100 ft	N/A	N/A
40 mph	625 ft	100 ft	N/A	N/A
45 mph	725 ft	125 ft	100 ft	100 ft
50 mph	825 ft	200 ft	175 ft	125 ft
55 mph	925 ft	275 ft	225 ft	125 ft
60 mph	1,025 ft	350 ft	275 ft	100 ft
65 mph	1,200 ft	425 ft	350 ft	200 ft
70 mph	1,250 ft	500 ft	425 ft	275 ft
75 mph	1,300 ft	575 ft	500 ft	350 ft

Figure 8B-7. Grade Crossing Pavement Markings

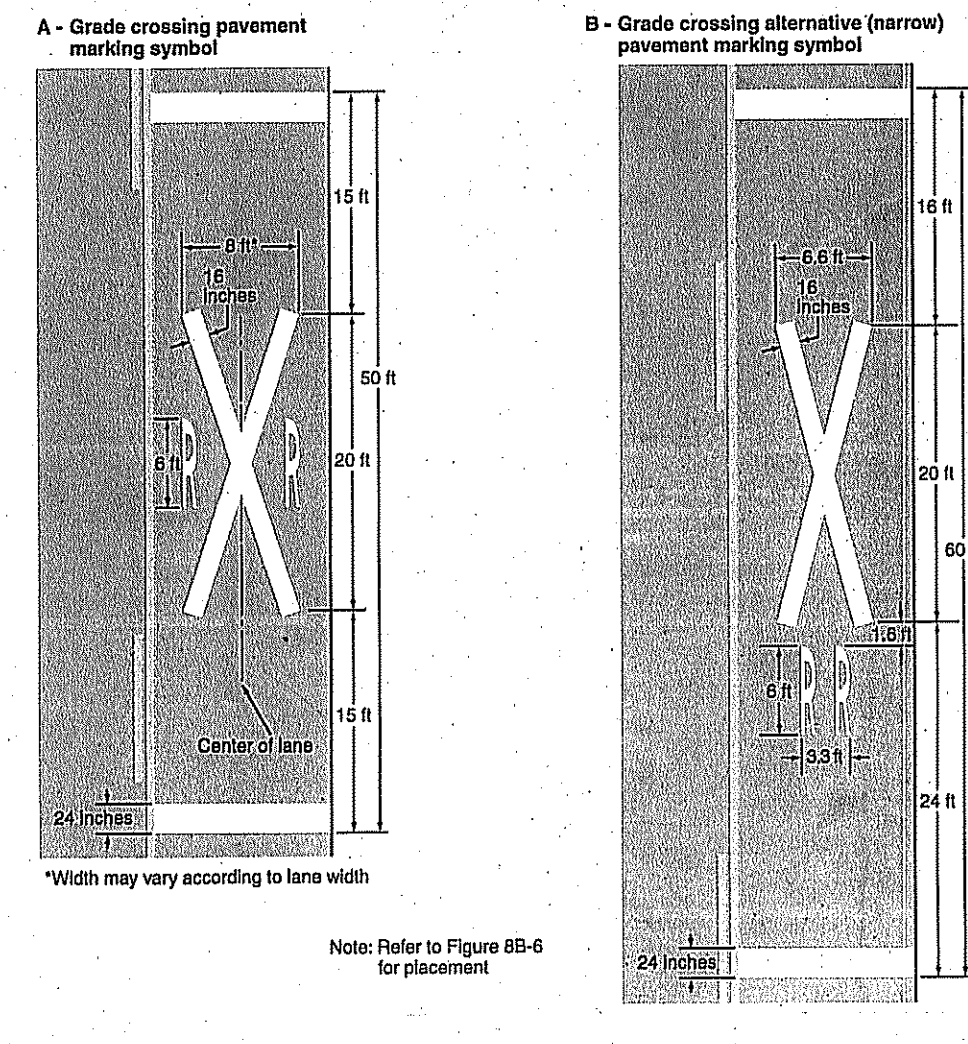
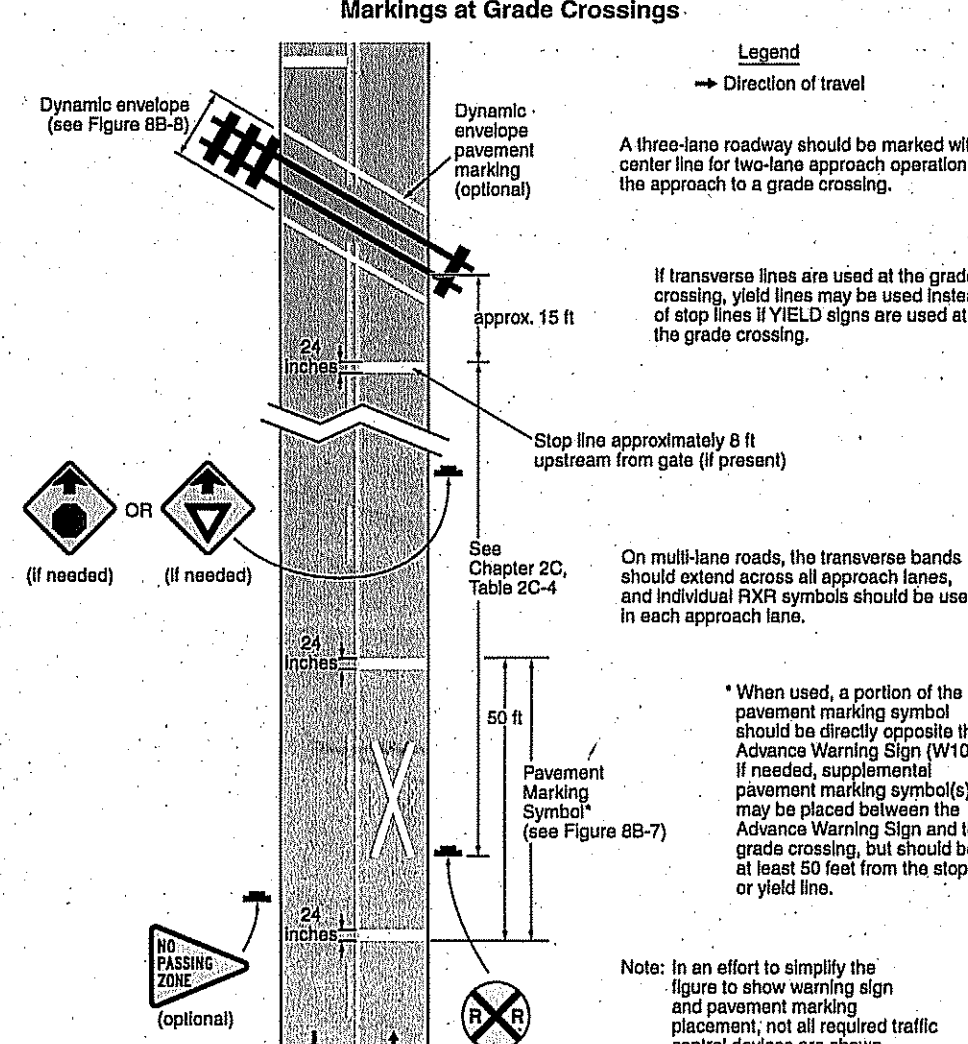


Figure 8B-6. Example of Placement of Warning Signs and Pavement Markings at Grade Crossings



**TOWN OF DOVER  
ENGINEERING DEPARTMENT**

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OFFICE: 100 PRINCETON AVENUE  
DOVER, NEW JERSEY 07801

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FAX: (973) 366-0039

Michael A. Hantson, PE, P.E., CME - Town Engineer  
William J. Iesolini - Assistant

**TRAFFIC CONTROL PLAN**

CONSTRUCTION PLANS FOR  
N. & S. BERGEN STREET  
ROADWAY IMPROVEMENTS  
TOWN OF DOVER  
MORRIS COUNTY, NEW JERSEY

PROJECT NO. 2018-01  
SCALE: N.T.S.

DATE: 7/24/18  
REVISION: REV. PER NJDOT COMMENTS

SHEET 6 OF 6  
REVISION: