MAYOR JAMES P. DODD

BOARD OF ALDERMAN

JAMES VISIOLI WILLIAM O'CONNOR MICHAEL PICCIALLO STEVEN TOTH THOMAS MacDONALD RONALD CAMACHO CAROLYN BLACKMAN

PUBLIC UTILITIES

NEW JERSEY NATURAL GAS. CO.

CINDY ROMAINE

TELEPHONE WATER TOWN OF DOVER WATER DEPARTMENT SANITARY SEWER TOWN OF DOVER SEWER DEPARTMENT ROCKAWAY VALLEY REGIONAL SEWER AUTHORITY SANITARY SEWER ELECTRIC

CABLEVISION

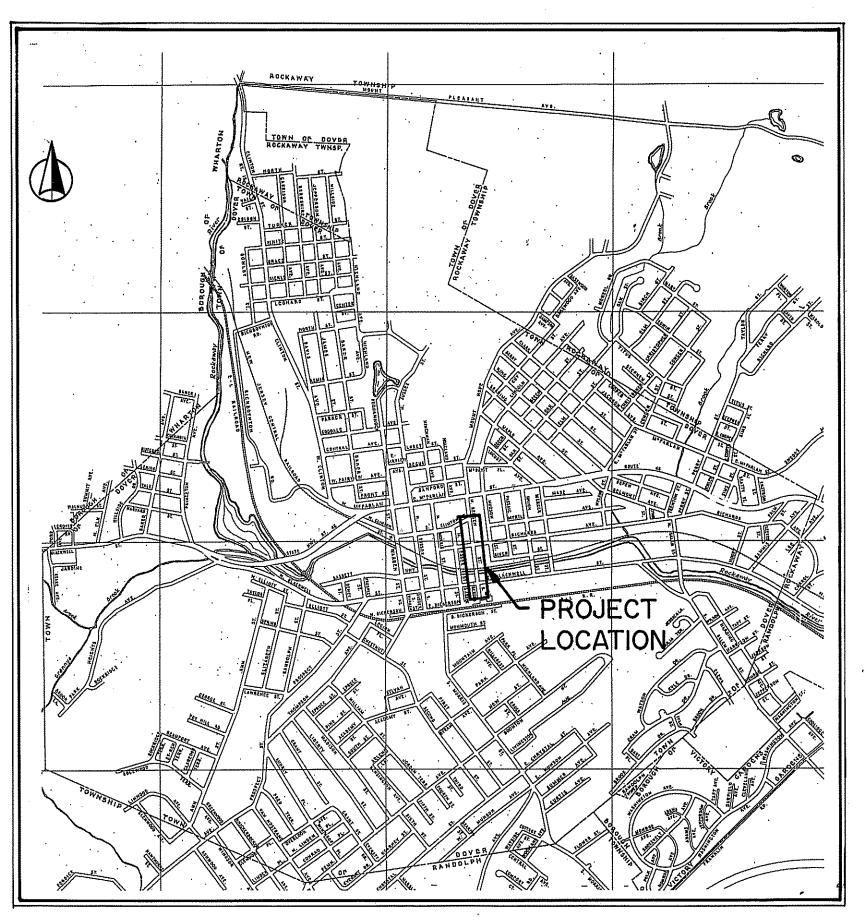
PRIOR TO ANY EXCAVATION, CONTRACTOR MUST HAVE ALL UTILITIES MARKED OUT BY THE APPROPRIATE UTILITY COMPANIES 1-800-272-1000

CABLE TV

| LIST OF QUANTITIES | | | | | | | |
|--------------------|-----------------|---------------|-------|---|--|--|--|
| ITEM NBR. | REFER. SECT. | UNIT MEAS. | QTY. | DESCRIPTION | | | |
| 1 | 159 | L.S. | 1 | Maintenance & Protection of Traffic | | | |
| 2 | 160 | L.S. | 1 | Asphalt Price Adjustment | | | |
| 3 | 201 | L.S. | 1 | Clearing Site | | | |
| 4 | 202 | C.Y. | 30 | Excavation Unclassified | | | |
| 5 | 302 | S.Y. | 112 | Dense Graded Aggregate Base Course, 4" Thk. | | | |
| 6 | 401 | S.Y. | 3,700 | HMA Milling, 3" or Less | | | |
| 7 | 401 | TON | 27 | HMA 19M64 Base Course, 4" Thk. | | | |
| 8 | 401 | TON | 500 | HMA 9.5 M64 Surface Course, 2" Thk. | | | |
| 9 | 601 | L.F. | 20 | 4" Sch. 40 PVC Leader Drain | | | |
| 10 | 602 | UNIT | 9 | Reconstruct Inlet Type "B" W/Bicycle Safe Grate & Curb Piece Type "N" Eco | | | |
| 11 | 602 | UNIT | 1 | Reconstruct Inlet Type "A" W/Bicycle Safe Grate | | | |
| 12· | 602 | UNIT | 3 | Reset Existing Casting | | | |
| 13 | 606 | S.Y. | 20 | HMA Driveway, 6" Thk. | | | |
| 14 | 606 | S.Y. | 350 | Concrete Sidewalks, 4" Thk. | | | |
| 15 | 606 | S.Y. | 75 | Concrete Sidewalks, 6" Thk. | | | |
| 16 | 606 | S.Y. | 16 | Detectable Warning Surface | | | |
| 17 | 607 | L.F. | 275 | Granite Block Curb | | | |
| 18 | 607 | L.F. | 225 | 9" x 18" Concrete Vertical Curb | | | |
| 19 | 610 | L.F. | 1,800 | Traffic Stripes, L.L. Epoxy Resin, 4" Thk. | | | |
| 20 | 610 | S. F. | 1,000 | Traffic Markings, Thermoplastic | | | |
| 21 | 612 | UNIT | 1 | Street Name Signs With Post | | | |
| 22 | 612 | S.F. | 20 | Regulatory & Warning Signs W/Posts | | | |
| 23 | 651 | UNIT | 1 | Reset Water Curb Box and Stop | | | |
| 24 | 651 | UNIT | 2 | Fire Hydrant Assembly, Connect to Exist. Gate Valve | | | |
| 25 | 804 | S.Y. | 100 | Topsoiling, 4" Thick | | | |
| 26 | 804 | S.Y. | 100 | Fertilizing & Seed, Type A-3 | | | |
| 27 | 809 | S.Y. | 100 | Seed Accelerator | | | |
| 28 | 903 | C.Y. | 2 | Miscellaneous Concrete | | | |
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CONSTRUCTION PLANS FOR 2018 NJDOT PROJECT N. & S. BERGEN STREET ROADWAY IMPROVEMENTS

TOWN OF DOVER MORRIS COUNTY, NEW JERSEY



KEY MAP

REVISED - AUGUST 2018

INDEX OF SHEETS

| SHEET NO. | DESCRIPTION | | |
|-----------|-----------------------|--|--|
| 1 | COVER SHEET | | |
| 2 | CONSTRUCTION PLAN | | |
| 3 | CONSTRUCTION DETAILS | | |
| 4 | CONSTRUCTION DETAILS | | |
| 5 | HANDICAP RAMP DETAILS | | |
| 6 | TRAFFIC CONTROL PLAN | | |
| | · | | |

GENERAL NOTES

- THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2007 EDITION, SHALL FORM THE BASIS OF THE CONTRACT. THIS SHALL BE AMENDED AND SUPPLEMENTED AS CONTAINED HEREIN AND ELSEWHERE IN THE
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT AND PROPER GRADING OF ALL

INSURANCE AND ALL ELSE NECESSARY FOR OR INCIDENTALS THERETO.

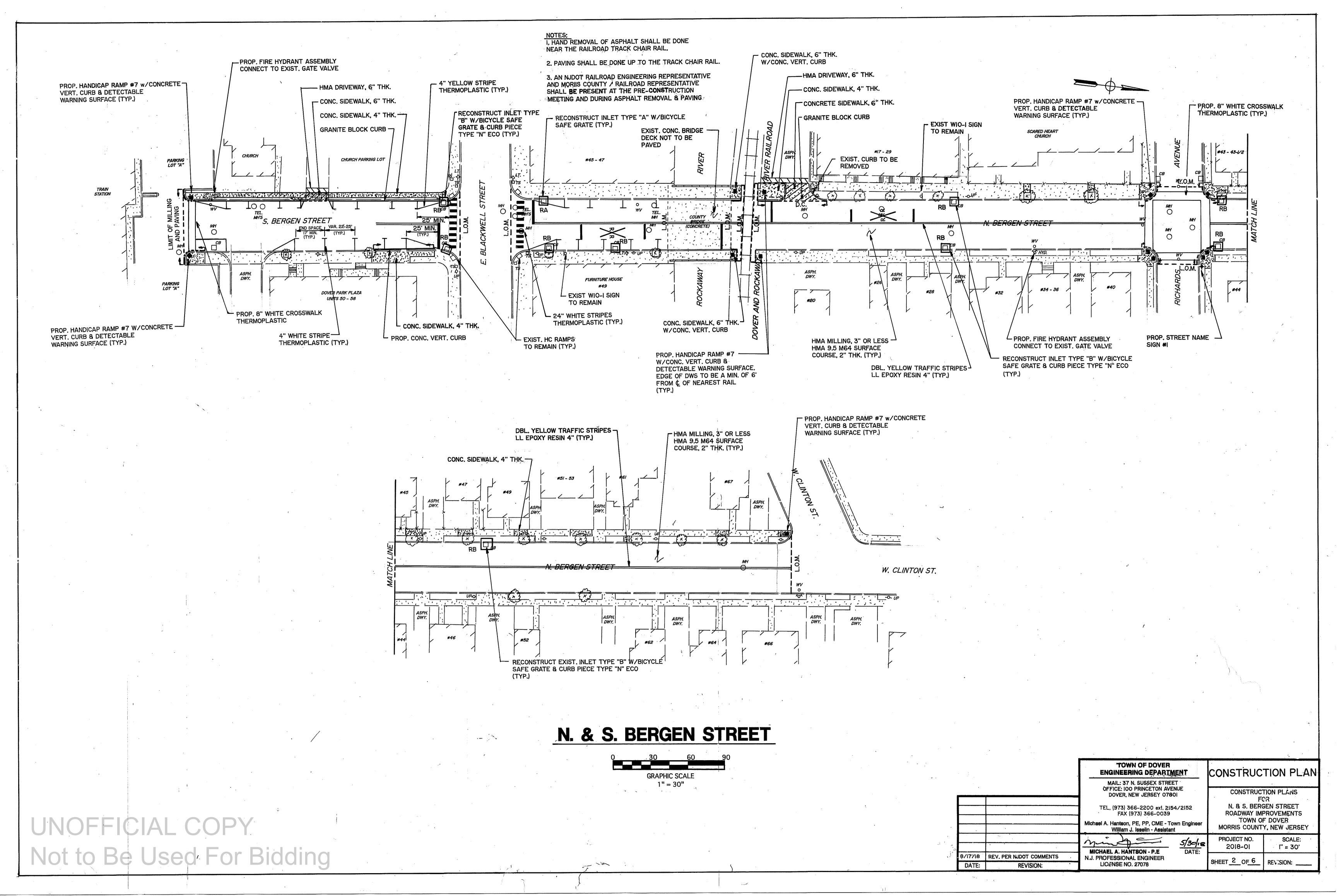
- THE CONTRACTOR SHALL PROVIDE ALL TOOLS, TRANSPORTATION, MATERIALS, LABOR,
- ADEQUATE WARNING SIGNS AND BARRICADES SHALL BE PLACED AROUND ALL EXCAVATIONS. FLASHING LIGHTS OR OTHER ILLUMINATION DEVICES SHALL BE EMPLOYED TO WARN MOTORIST AND PEDESTRIANS AFTER DARK. ALL TRAFFIC CONTROL MEASUREMENTS SHALL MEET THE REQUIREMENTS OF THE NJDOT REGULATIONS AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" 2009 EDITION WITH ANY UPDATES.
- ALL EXISTING SIGNS SHALL BE REMOVED AND RESET DURING CONSTRUCTION. THIS WORK SHALL BE INCLUDED UNDER THE BID ITEM "CLEARING SITE".
- ALL EXISTING VALVE BOXES SHALL BE REMOVED, AND OR RESET TO PROPOSED GRADE DURING CONSTRUCTION. THIS WORK SHALL BE INCLUDED UNDER THE BID ITEM "CLEARING SITE". THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR RESETTING OF ALL SAID BOXES AND FOLLOW THEIR INSTRUCTIONS IN THIS REGARD.
- WORK HOURS FOR MONDAY THROUGH FRIDAY SHALL BE FROM 8:00AM 5:00 PM. WORK ON SATURDAYS, SUNDAYS OR LEGAL HOLIDAYS IS PROHIBITED UNLESS PERMISSION IS GRANTED
- THE EXISTENCE OF UNDERGROUND UTILITIES IS KNOWN IN THE PROJECT AREA. THE CONTRACTOR SHALL NOT PROCEED WITH ANY EXCAVATION UNTIL HE HAS CONTACTED EACH OF THE UTILITY COMPANIES AND MUNICIPAL AUTHORITIES TO DETERMINE THE EXACT LOCATION OF ALL UTILITIES. THIS SHALL INCLUDE THE "MARKING OUT" OF PHYSICAL LOCATION
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF ALL WASTE MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS.
- THE OWNER RESERVES THE RIGHT TO INCREASE OR DECREASE QUANTITIES AND TO ADD OR DELETE ITEMS AS REQUIRED.
- EXISTING INLET CASTINGS REMOVED SHALL REMAIN THE PROPERTY OF THE TOWN OF DOVER. THE CONTRACTOR SHALL DELIVER ALL CASTINGS TO THE DEPARTMENT OF PUBLIC WORKS
- THE CONTRACTOR SHALL MAINTAIN SAFE ACCESS TO THE EXISTING DWELLINGS AND SHALL BE MAINTAINED DURING THE ENTIRE TIME OF CONSTRUCTION
- WHENEVER THE DRAWINGS INDICATE TO MEET EXISTING CONDITIONS, THE CONTRACTOR SHALL PROVIDE FOR A SMOOTH AND CONTINUOUS TRANSITION FROM PROPOSED TO EXISTING
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS. ADDITIONAL CONSTRUCTION
- THIS PLAN DOES NOT CERTIFY TO THE LOCATION, BOTH HORIZONTAL AND VERTICAL OF ANY
- UNDERGROUND FEATURE WHICH HAS NOT BEEN EXPOSED FOR DIRECT MEASUREMENT THE PLANIMETRIC FEATURES ON THIS MAP ARE A COMBINATION OF EXISTING TOWN TOPOGRAPHY MAPS DATED 1978 AND A FIELD INVESTIGATION, LOCATION OF ALL PHYSICAL FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION, ALL
- NO MATERIAL OR EQUIPMENT IS TO BE STORED ON ANY STREET OR RIGHT OF WAY OVERNIGHT WITHOUT THE PERMISSION OF THE TOWN ENGINEER. ALL EXCAVATIONS SHALL BE BACKFILLED AT THE END OF EACH WORKDAY, ALL EQUIPMENT AND MATERIALS SHALL BE STORED IN ACCORDANCE WITH THE TOWN ENGINEER AND THE SUPERINTENDENT OF THE STREET

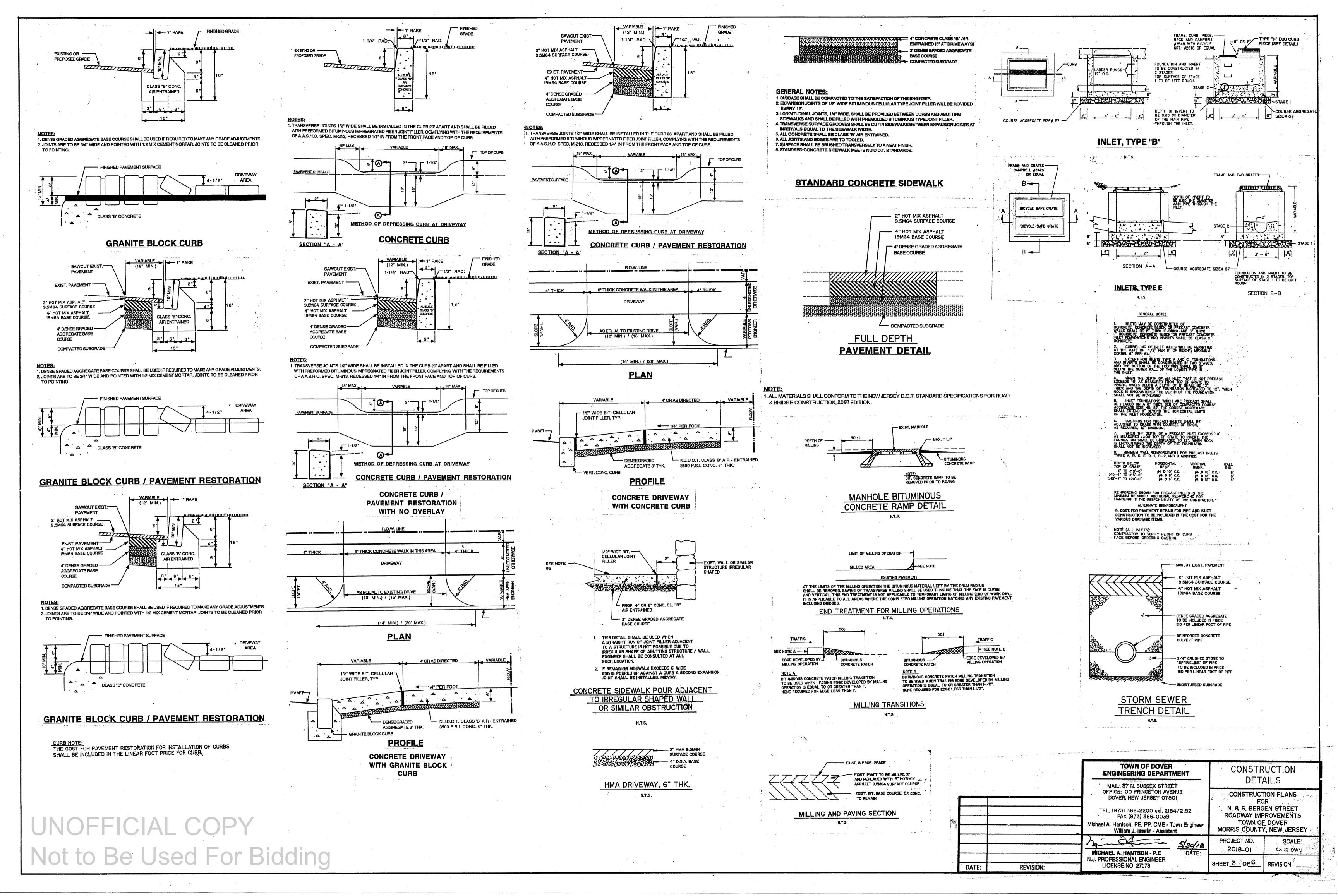
INFORMATION AND REPORT ALL DISCREPANCIES AT THE TIME OF BID.

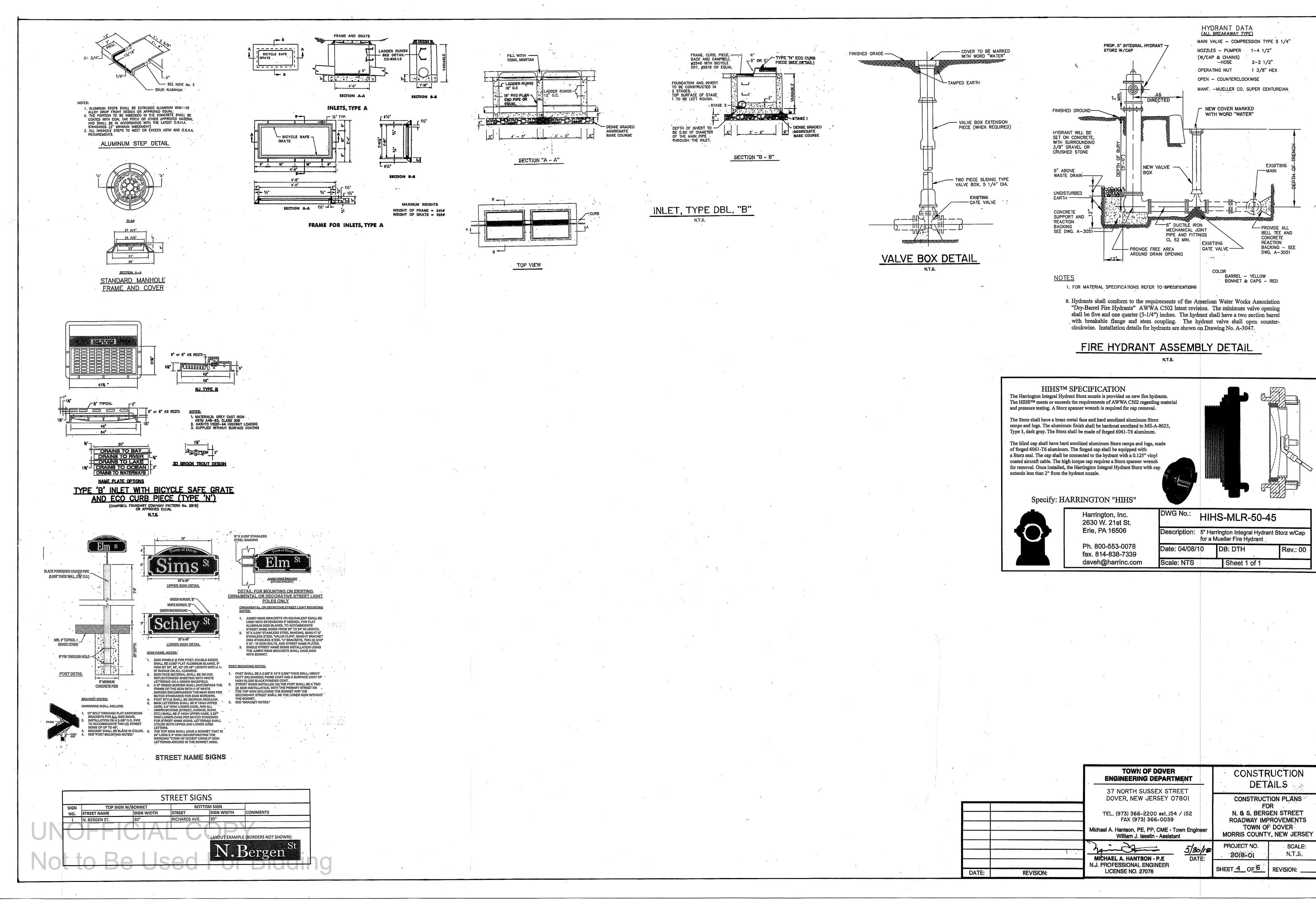
- 18. THE CONTRACTOR SHALL PROVIDE ACCESS FOR AND COORDINATE WITH THE TOWN OF DOVER'S SOLID WASTE & RECYCLING HAULERS TO INSURE ACCESS FOR PICK-UPS ON NORMALLY SCHEDULED PICK-UP DAYS. FAILURE TO PROVIDE ACCESS WILL RESULT IN A PAYMENT ADJUSTMENT FOR "MAINTENANCE OF TRAFFIC"
- 19. THE LOCATION OF ALL UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
- THE LOCATION OF THE PROPOSED DRAINAGE IS APPROXIMATE AND IS SUBJECT TO SLIGHT VARIATIONS BASED ON EXISTING FIELD CONDITIONS OR CONFLICTS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR RELOCATING PROPOSED DRAINAGE FACILITIES WITHOUT EXPRESS PERMISSION FROM THE ENGINEER.
- THE ELEVATIONS SHOWN ARE BASED ON TOWN OF DOVER TOPOGRAPHIC MAPS AND FIELD OBSERVATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL ELEVATIONS PRIOR TO CONSTRUCTION FOR POSSIBLE UTILITY CONFLICTS.
- THE CONTRACTOR SHALL COORDINATE DIRECTLY WITH ALL UTILITY COMPANIES FOR THE RELOCATION OF ANY UTILITY MAINS OR SERVICES, WHICH MAY BE IN CONFLICT OF TH PROPOSED DESIGN. NO ADDITIONAL COMPENSATION WILL BE MADE FOR TIME LOST DUE TO
- ALL GRATE ELEVATIONS ARE APPROXIMATE, ALL GRATES SHALL BE SET AT OR SLIGHTLY BELOW THE EXISTING PAVEMENT ELEVATION TO ASSURE PROPER GUTTER FLOW.
- DEWATERING OPERATIONS SHALL BE INCLUDED IN THE BID PRICES FOR ALL ITEMS IN THE CONTRACT. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK,
- STANDARD PLUMBING CODES.
- 26. THE CONTRACTOR IS RESPONSIBLE TO INSURE THAT ANY TRENCH / EXCAVATION CONFORM IN ALL RESPECTS TO O.S.H.A. REGULATIONS FOR EXCAVATING, TRENCHING AND SHORING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING ALL ACTIVE SANITARY SEWER LATERALS INTO THE NEW PVC PIPE. THE CONNECTIONS SHALL BE IN ACCORDANCE WITH THE CONSTRUCTION DETAILS PROVIDED. LATERAL CONNECTION SHALL BE PAID UNDER THE
- 28. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL SAFEGUARDS NECESSARY TO PROTECT THE PUBLIC SAFETY AND ADJOINING PROPERTIES.
- THE CONTRACTOR SHALL FURNISH, PLACE AND MAINTAIN SHEETING, BRACING, SHORING AND OTHER SUPPORTS REQUIRED BY THE WORK. NO SEPARATE PAYMENT WILL BE MADE FOR
- 30. NO SEPARATE PAYMENT WILL BE MADE FOR PAVEMENT CUTTING WITH A SAW AND THE COST THERETO SHALL BE INCLUDED IN THE VARIOUS ITEMS INCLUDED IN THE PROPOSAL. JACKHAMMER CUTS ARE NOT PERMITTED FOR A FINISHED EDGE.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO EXISTING DRIVEWAYS AT THE END OF EACH DAY. DISRUPTION TO PROPERTIES SHALL BE KEPT TO A MINIMUM,
- 32. HORIZONTAL AND VERTICAL CONTROL WILL NOT BE PROVIDED PRIOR TO CONSTRUCTION, ALL
- CONTROL WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AT NO ADDITIONAL COST.
- 33. ALL TRENCHES SHALL BE STABILIZED AT THE END OF EACH DAY AS PER PROVIDED
- 34. ALL PROPERTY CORNERS OR MONUMENTS REMOVED DURING CONSTRUCTION SHALL BE REPLACED BY A NEW JERSEY LICENSED LAND SURVEYOR, AT NO SEPARATE COST TO THE
- 35. ALL PAVEMENT RESTORATION ASSOCIATED WITH THE INSTALLATION OF NEW CURBING SHALL BE INCLUDED IN THE PRICE PER LINEAR FOOT OF CURB.
- ALL BACKFILL AND PAVEMENT RESTORATION ASSOCIATED WITH THE INSTALLATION OF PROPOSED DRAINAGE SHALL BE INCLUDED IN THE COST PER LINEAR FOOT OF PIPE.
- THE CONTRACTOR SHALL INSPECT ALL ROADWAYS AFTER MILLING AND PRIOR TO PAVING TO INSPECT THE ROADWAY FOR SOFT SPOTS OR AREAS OF POOR SUBGRADE. PRIOR TO PAVING ALL AREAS OF POOR SUBGRADE SHALL BE IDENTIFIED AND PROPERLY RECONSTRUCTED WITH DGA AND ASPHALT BASE MATERIAL. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPAIR ALL AVMENT FAILURES AFTER THE SURFACE COURSE OF PAVEMENT HAS BEEN INSTALLED FOR THE LENGTH OF THE MAINTENANCE BOND PERIOD AT NO ADDITIONAL COST TO THE
- 38. ANY CONTRACTOR(S) OR ANY OF THEIR SUBCONTRACTOR(S) WORKING AROUND THE RAILROAD TRACKS AND/OR WITHIN THE RAILROAD'S R.O.W. AND/OR ADJACENT TO THEIR FACILITIES WILL BE SUBPART A. THE INSURANCE POLICY SHALL BE IN THE NAME OF CHESAPEAKE & DELAWARE, LLC., AND ALL OPERATING RAILROADS ON THAT LINE, WITH A LIMIT OF NOT LESS THAN \$2,000,000.00 PER OCCURRENCE, AS COMBINED SINGLE LIMIT FOR BODILY INJURY AND/OR PROPERTY DAMAGE AND \$6,000,000.00 PER ANNUAL AGGREGATE FOR DAMAGES ARISING OUT OF MORE THAN ONE
- 39. SINCE THE PROPOSED WORK AS IS WITHIN 15' OF THE NEAREST RAIL, AN AUTHORIZED REPRESENTATIVE/FLAGMAN FROM THE DOVER AND ROCKAWAY RIVER RAILROAD/CHESAPEAKE & DELAWARE, LLC., MAY BE REQUIRED TO BE PRESENT AT ALL TIMES DURING THE CONSTRUCTION. TO ARRANGE FOR A FLAGMAN. THE CONTACT INFORMATION FOR DOVER AND ROCKAWAY RIVER RAILROAD/CHESAPEAKE & DELAWARE, LLC., IS KEAN BURENGA 833-752-1849 X 704 EMAIL IS
- 40. IN CASE OF AN EMERGENCY DURING CONSTRUCTION INVOLVING THE AREA IN AND/OR AROUND THE RAILROAD TRACKS OR THE RAILROAD WARNING DEVICES, THE EMERGENCY CONTACT INFORMATION FOR THE DOVER AND ROCKAWAY RIVER RAILROAD/CHESAPEAKE & DELAWARE, LLC.

| | | | TOWN OF DOVER ENGINEERING DEPARTMENT | COVER | SHEET |
|-------|-------------------------|-----------------------------------|---|---|------------------|
| | | | MAIL: 37 N. SUSSEX STREET OFFICE: 100 PRINCETON AVENUE DOVER, NEW JERSEY 07801 TEL. (973) 366-2200 ext. 2154/2152 FAX (973) 366-0039 | CONSTRUCTION PLANS FOR N. & S. BERGEN STREET ROADWAY IMPROVEMENTS TOWN OF DOVER | |
| , , , | , , | | Michael A. Hantson, PE, PP, CME - Town Engineer William J. Isselin - Assistant | MORRIS COUNTY, NEW JERSEY | |
| | | | MICHAEL A. HANTSON - P.E DATE: | PROJECT NO. 2018-01 | SCALE: N.T.S. |
| | 6/17/18 DATE: | REV. PER NUDOT COMMENTS REVISION: | N.J. PROFESSIONAL ENGINEER LICENSE NO. 27078 | SHEET 1 OF 6 | REVISION: |

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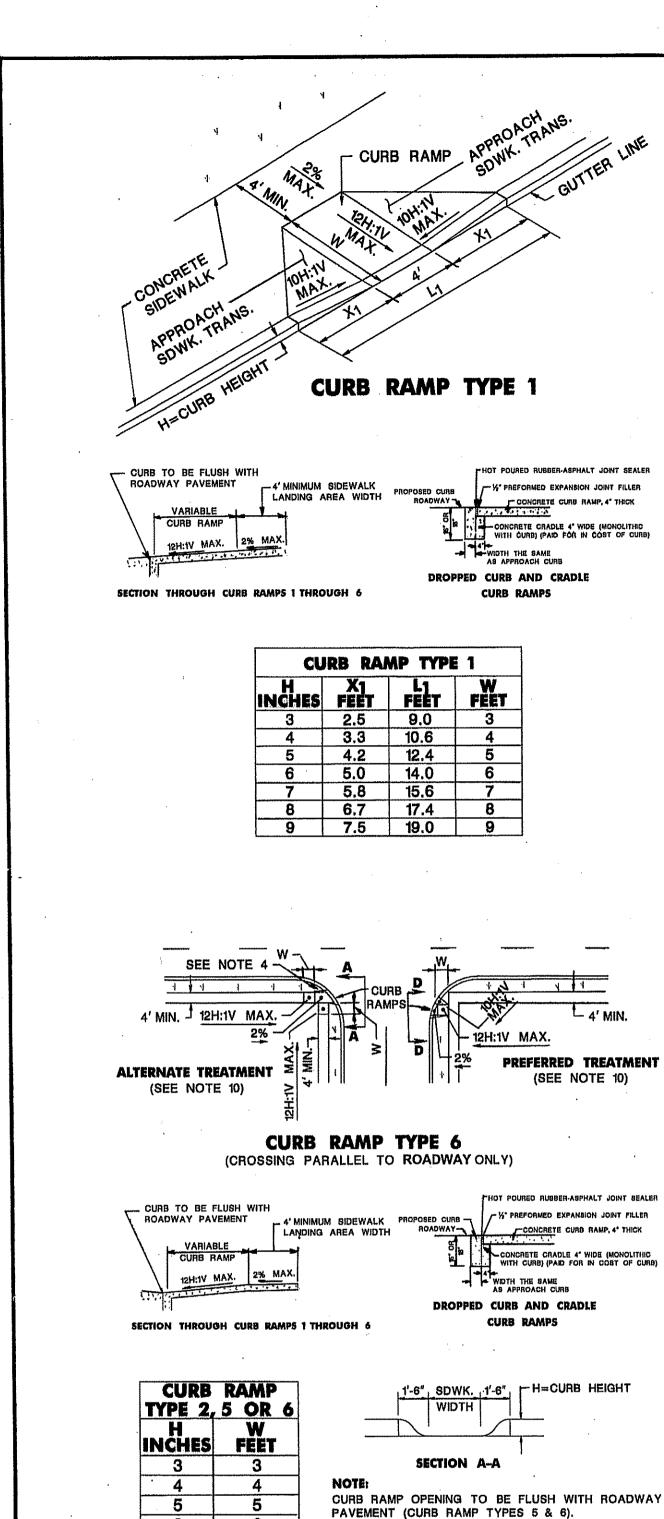


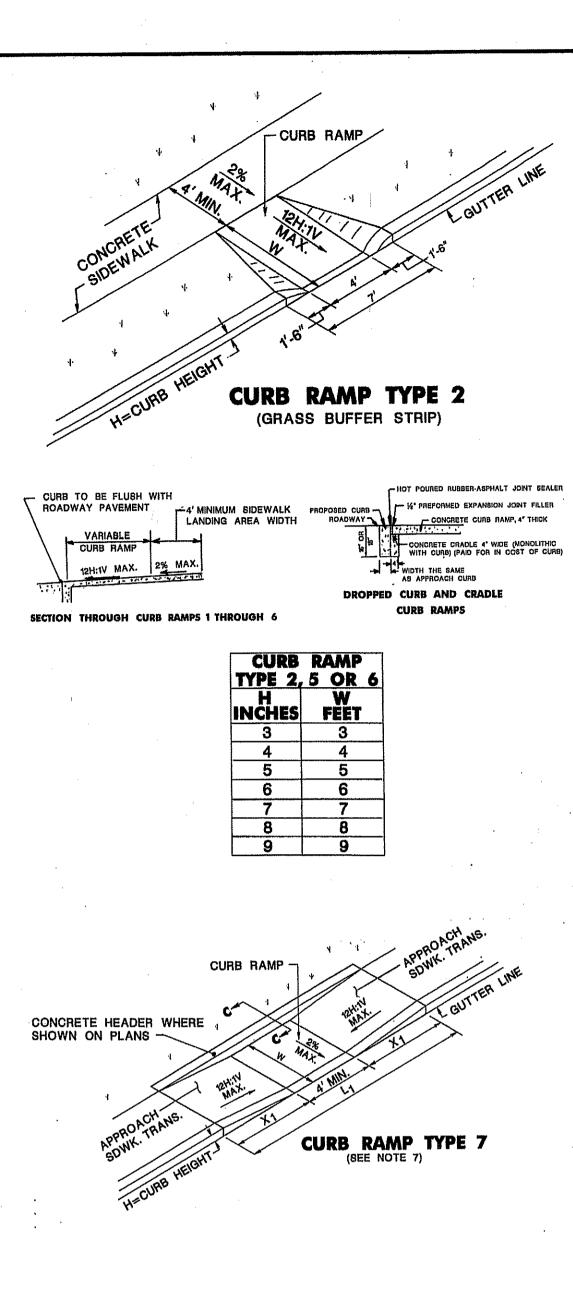


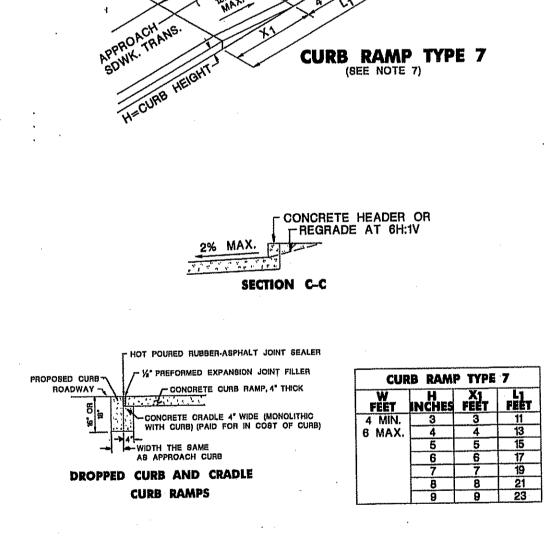


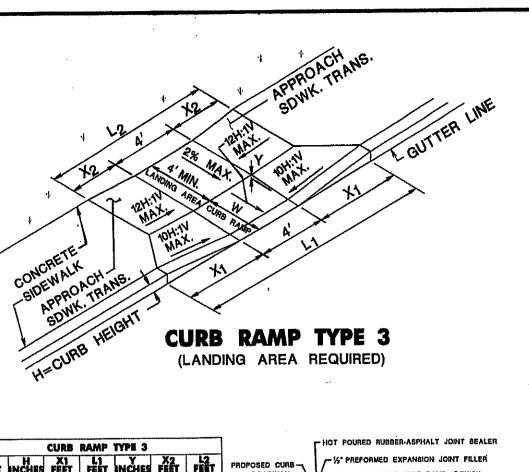
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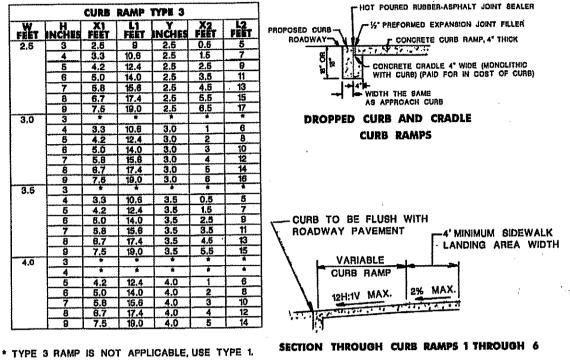
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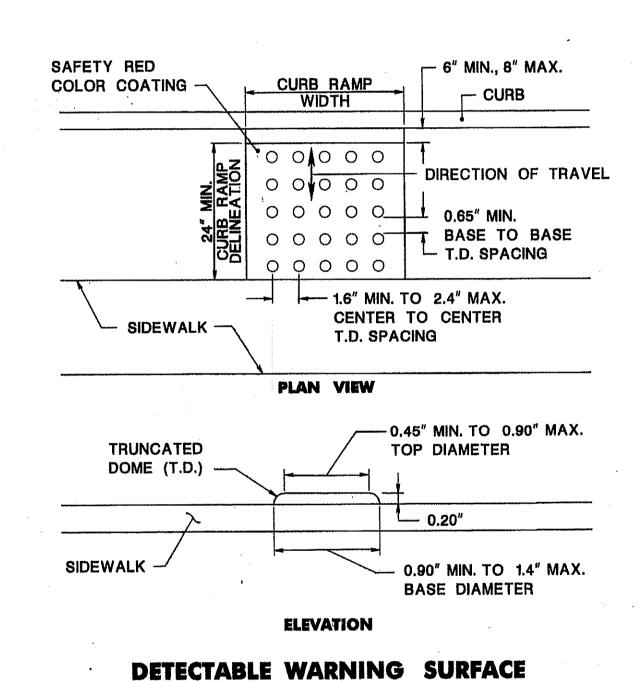


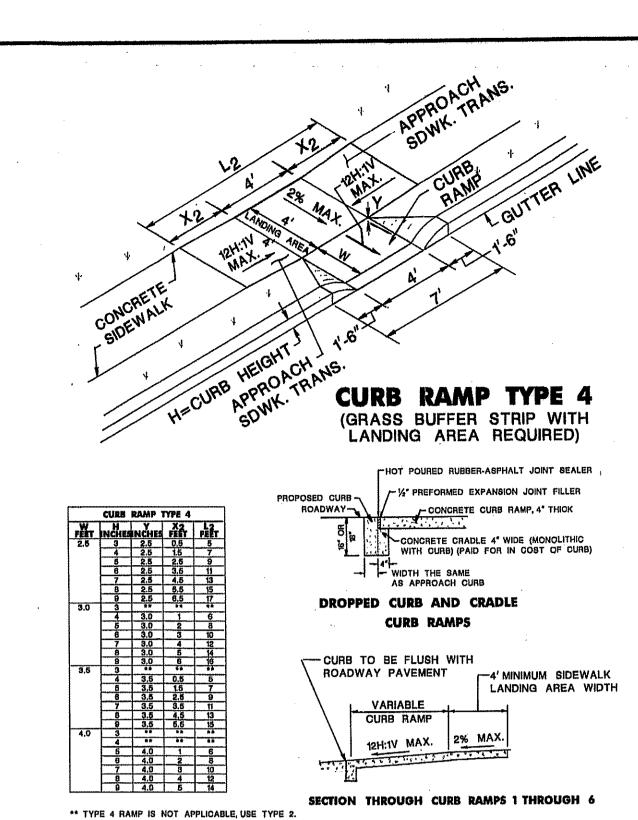


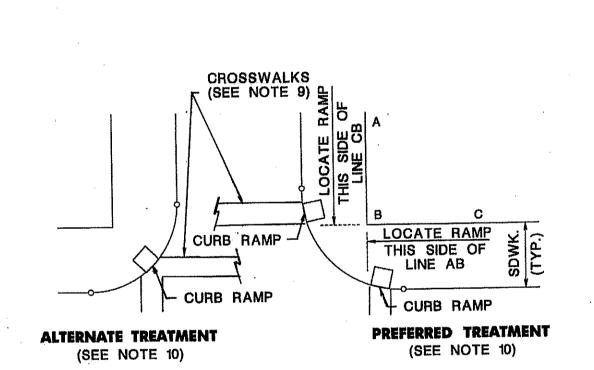




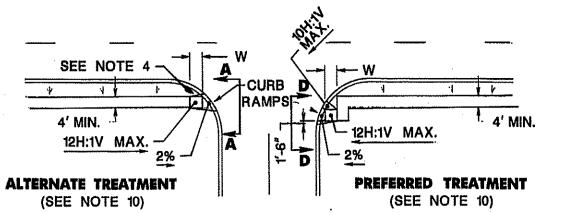




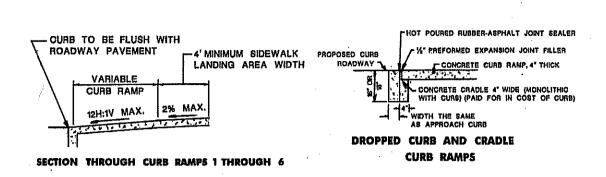


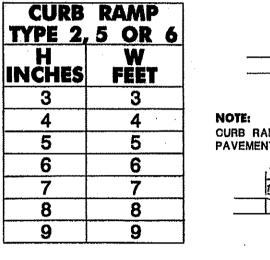


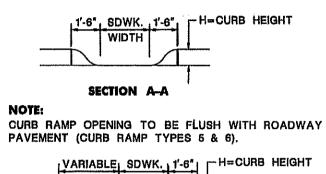
LOCATION OF CURB RAMP TYPES 1, 2, 3, 4 & 7 FOR CROSSING PARALLEL AND PERPENDICULAR **TO ROADWAY**



CURB RAMP TYPE 5 (CROSSING PARALLEL TO ROADWAY ONLY)





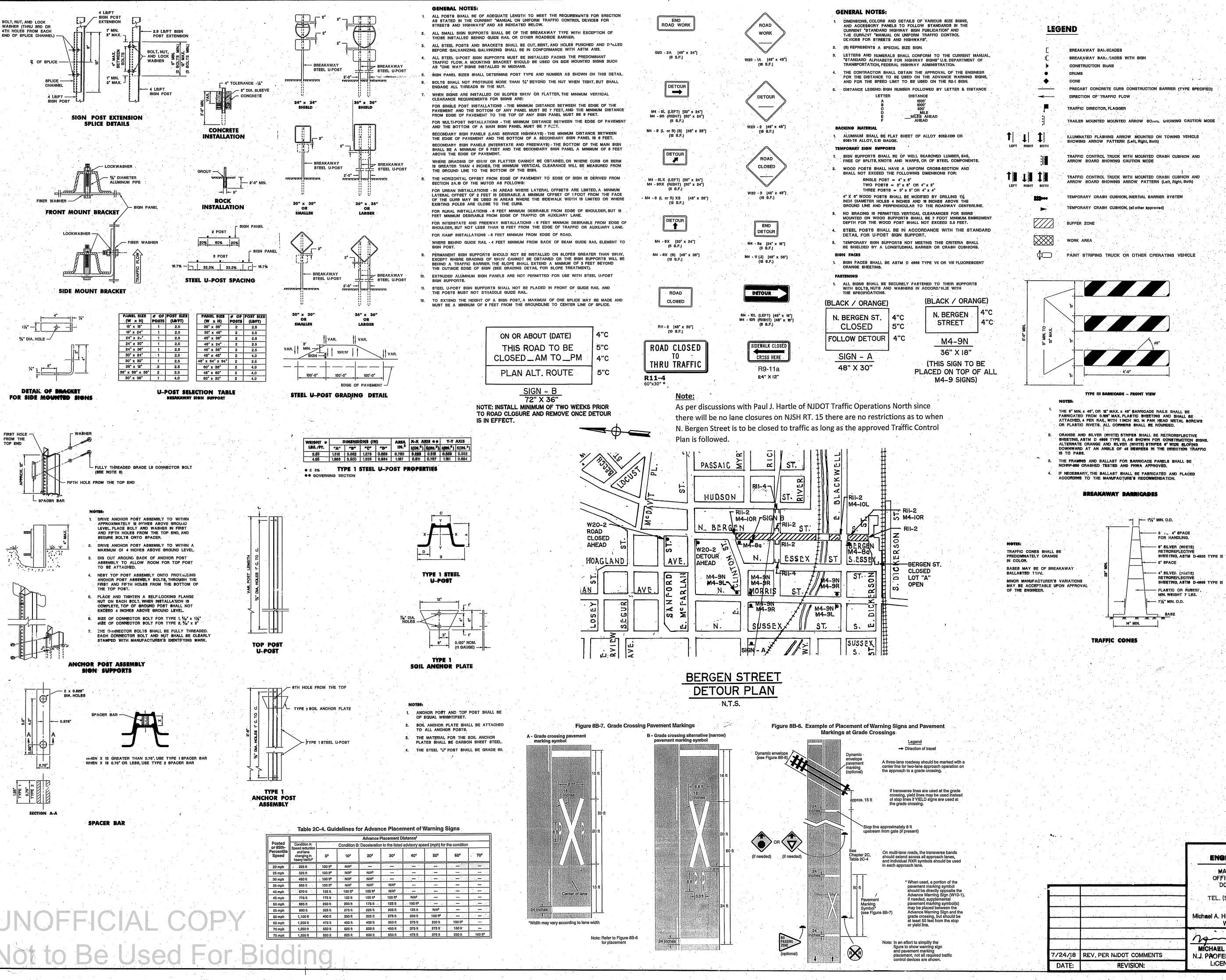


- 1. LANDING AREA, APPROACH SIDEWALK TRANSITIONS, AND CURB
- CURB RAMP SIZE BASED ON CURB HEIGHT (H) WHERE ELEVATION
- PROPOSED CURB RAMP. 3. CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY
- 4. FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT
- EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
- LINES TO BE PAID FOR AS CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
- 6. CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS VERTICAL CURB OR SLOPING CURB OF THE
- APPROPRIATE ADJACENT SIZE AND KIND. 7. WHERE THE DISTANCE FROM THE GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 6 FEET OR LESS, CURB RAMP TYPE 7 SHOULD
- BE USED, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4. 8. THE PUBLIC SIDEWALK CURB RAMP, DETECTABLE WARNING SURFACE (SHADED AREA) SHALL BE SAFETY RED COLOR ON
- CONCRETE OR 70% COLOR CONTRAST FOR OTHER SURFACE SUCH
- 9. CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED, SEE PLANS.
- 10. PREFERRED AND ALTERNATE TREATMENTS SHOULD NOT BE INTERMIXED WITHIN THE SAME INTERSECTION.
- 11. DIMENSIONS SHOWN IN TABLES ARE FOR 3 INCH TO 9 INCH CURB HEIGHTS, WHERE THE CURB HEIGHTS ARE OTHER THAN WHAT IS PROVIDED IN THE TABLES, THE DIMENSIONS OF THE RAMPS WILL HAVE TO BE CALCULATED BASED ON CROSS SLOPES SHOWN.

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SECTION D-D

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|-----------------|--|---|--|
| | TOWN OF DOVER ENGINEERING DEPARTMENT | HANDICAP RAMP DETAILS | |
| | MAIL: 37 N. BUSSEX STREET | DE 17 | 1154 |
| | OFFICE: 100 PRINCETON AVENUE DOVER, NEW LERSEY OTSO! | CONSTRUCTION PLANS FOR | |
| | FAX (973) 366-2200 ext, 2164/2152 ROADWAY | | ERGEN STREET IMPROVEMENTS |
| | — Michael A. Hantson, PE, PP, CME - Town Engineer William J. Isselin - Assistant | TOWN OF DOVER MORRIS COUNTY, NEW JERSEY | |
| | b - 40 = -1-1 | PROJECT NO. | SCALE: |
| | 5/30/18 | 2018-01 | N.T.S. |
| DATE: PEVISION: | MICHAEL A. HANTSON - P.E DATE: N.J. PROFESSIONAL ENGINEER LICENSE NO. 27078 | SHELT 5 OF 6 | REVISION: |



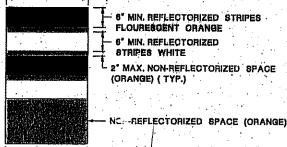
GENERAL NOTES:

- ADVANCE WARNING SIGNS DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
- THE APPROXIMATE LOGATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS, THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL. CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION, ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
- S. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIG CONTROL SIGNS AND DEVICES SHALL BE IN PLACE. RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE WIRD FIGN (ROAD WORK AHEAD) AS A MINIMUM.
- ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS AND/OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN BHALL BE COVERED, REMOVED OR RELOCATED AS DIRECTED BY THE RE.
- CONFLICTING OF NON-OPERATING BIGNAL MOIOATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC BIGNAL SYSTEMS SHALL BE BAGGED OR
- MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PART VI"STANDARDS AND QUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.
- CONSTRUCTION SIGN W89-2 (SIVE US A SHAKE) SHALL BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
- A WI-6 (ARROW) BIGN MOUNTED ON A BREAKAWAY BARRIGADE AND CENTERED ON THE CLOSED WIDTH SHALL BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
- D. CONSTRUCTION SIGNS RI14 (ROAD CLOSED TO THRU TRAFFIC) SHALL BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BEGAUSE OF
- 11. CONSTRUCTION SIGNS WS-9A (SYMBOL FOR UNEVEN PAVEMENT) AND WS-14A (GROOVED PAVEMENT) SHALL BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS SHALL BE AS DIRECTED BY THE RE.
- 12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRALER MOUNTED ELLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT SHALL MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN AND 250 FEET MAX BUFFER IN ADVANCE OF EACH WORK AREA.
- 13. THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES, THE PLAN SHALL BE SUBMITTED TO THE RE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- N. TRAFFIG BAFETY BERVICES SHALL BE USED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL.
- 18. ALL EXCAVATED AREAB WITHIN OR ADJACENT TO THE ROADWAY BHALL BE BACKFILLED AND PLACED ON AT LEAST SH :19 SLOPE BEFORE THE END OF EACH WORK DAY, OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE SHALL BE BACKFILLED.
- 16. WHERE REQUIRED, THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.
- BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES SHALL BE TRANSITIONED ON A MINIMUM 20H : 1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
- THE PLACEMENT AND OR RELOCATION OF PRECAST CONCRETE CURB, CONSTRUCTION BARRIER SHALL BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.
- 19. CONSTRUCTION ZONE SPEED LIMIT WILL SE DETERMINED BY THE TRAFFIC SIGNAL A SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER WORK ZONE, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED, BY THE R.E.,
- 20. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) BIGNS SHALL BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE TRAFFIC BIGNAL & SAFETY ENGREERING REGIONAL TRAFFIC ENGINEER WORK ZON 21. THE REDUCED SPEED AHEAD SIGN, WS-8(8) (BLACK ON ORANGE) SHALL BE LOCATED IN ADVANCE OF SPEED LIMIT RE-1 BIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
- 22. THAFFIC FINES DOUBLED IN WORK AREA RINJS-17(8), 4 FEET BY 2.5 FEET SIGN SHALL BE LOCATED SOO FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS, THIS SIGN SHALL ALSO BE USED ON PROJECTS REQUIRING HOVING OPERATIONS IN WHICH CASE THE SIGN SHALL BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
- 23. THE FINAL HMA SURFACE PAVEMENT BHALL NOT BE CONSTRUCTED UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. MANIFOLES AND INLETS SHALL BE BET TO FINISHED BRADE AND TEMPORARY PAVEMENT RAMPS ARE TO BE CONSTRUCTED AROUND THEM WITH A MINIMUM 20H : IV SLOPE IN ALL DERICTIONS USING HOT MIX ASPIALT PAVEMENT, THIS, TEMPORARY MATERIAL WILL BE REMOVED MAMERIATELY PRIOR TO PLACING THE BUFFACE COURSE.
- TRAFFIO CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, GONES, BARRICADI ETC. SHALL SE PLAGED AS SHOWN ON PLANS, SIGNS SHALL NOT SE FLAGED VITHOU ACTUAL LANE CLOSURES AND SHALL SE MIMEDIATELY NEMOVED UPON REMOVAL OF
- 25. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE R
- , TERMS: THE THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING SHALL BE AS FOLLC://S:
- IL PERMANENT LANE CLOBURES WORK DESCRIBED UNDER "IMPACTO TO NORMAL \HAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.
- FOR THE INITIAL START OF WORK THAT REQUIRES "MPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR SHALL NOTIFY THE RE IN WHITING, ON THE ADVANCE FORM TO DO PROVIDED BY THE OBPARTMENT, OF THE FROPPEED DATE, THE MOTTICE SHALL HE ENDIRITED AT LEAST

- AIL 'IMPACTS TO NORMAL TRAF. "LOW' SCHEDULED FOR THE SEVEN DAY PERIOD STARTING, ON THE ILLEWING MONDAY SHALL BE SLIMITED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FOR. TO THE PROVINCE BY THE DEPARTMENT. EACH DAY OF "TEMPORARY LANE (LOBURES" SHALL BE SUBMITTED TO THE RE BY 8:00 AM THE C Y IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO 02 PROVIDED BY THE DEFA. MENT.
- "TEM VITTY LAME OLOSURES" FOR WEEKENDS SHALL BE BUBMITTED TO THE RE BY 9:00 AM ON THE MINEDIATELY PRECEDENS FREDRY ON THE DAILY FORM TO 102 PROVIDED BY THE DEPARTMENT.
- D. CHANGES TO THE SCHEDULED CLOSURES
 REGISTET FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS 8,1 LL.
 BE SUBMITTED IN WAITING TO THE RE AS FOLLOWS: CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RE AT LEAST BIGHT GALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START. OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" SHALL BE SUBMITTED TO THE ME AS SPECIFIED BY THE OPECIFICATIONS.

DRUMS SHALL BE MADE OF ORANGE PLASTIC WITH A MINIMUM OF FOUR ALTERNATE FLUORESC NY ORANGE AND WHITE RETROREFLECTIVE STRIPES IF THERE ARE NON-REFLECTIVE SPACES BETWEEN THE STRIPES, THEY SHALL BE NO MORE THAN 2" WIDE RETROREFLECTIVE SHUETING FOR

PROVIDE THE SAME VISIBILITY AS AN 16 INCH DIAMETER ROUND DRUM REGARDLESS OF ORIENTATION, MAY BE USED.



STRIPES WHITE 2" MAX. NON-REFLECTORIZED SPACE

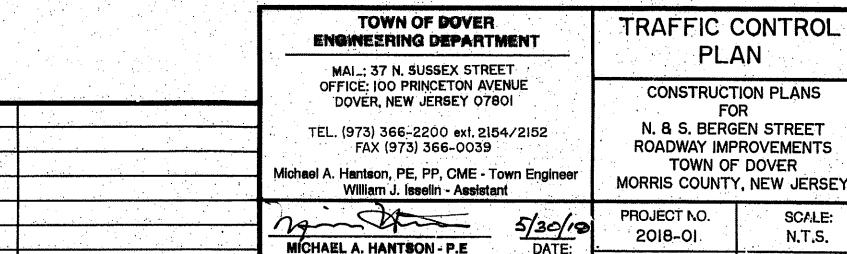
SHEET 6 OF 6

REVISION:

WHEN BALLAST IS REQUIRED BY THE R.E., SAND SHALL BE USED. THE MAXIMUM WEIGHT OF THE BALLAST SHALL BE 50 LBS. AND BE LOCATED APPROXIMATELY AT GROUND LEVEL. ALTERNATE TYPES OF BALLAST SHALL BE APPROVED BY THE R.E.,

DRUMS

DATE:



N.J. PROFESSIONAL ENGINEER

LICENSE NO. 27078